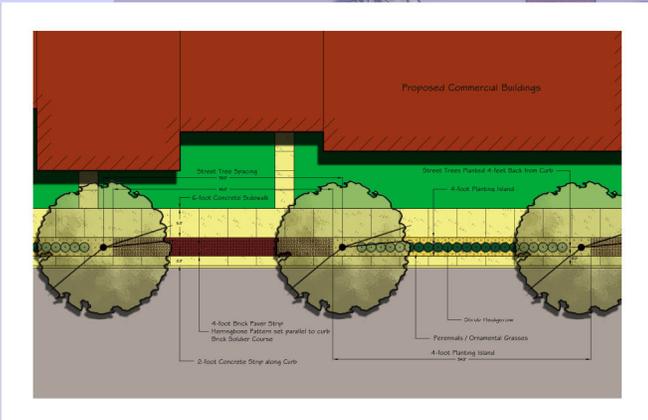
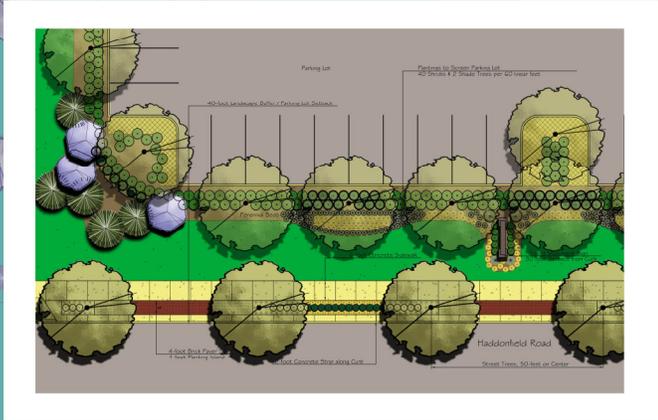


Haddonfield Road Study Cherry Hill Township New Jersey



New Shared Access Driveway



April 2008



Graham Av

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Appendix A

1.0 PURPOSE

The purpose of the Haddonfield Road Study is threefold. First, the study will consider the viability of mobility improvements along the study area. Second, design guidelines will be recommended to help create an aesthetically pleasing and truly walkable environment. Third, recommendations will be developed for improving access management into and out of the businesses to ensure the safety for all road users, including cars, cyclists, and pedestrians. The three components of the study are intrinsically linked to each other, and the vision for Haddonfield Road cannot be realized unless each component is executed. All three components work together to help establish a seamless transition from the pedestrian-oriented Towne Center at Garden State Park across Haddonfield Road and into the existing Locustwood neighborhood.

The study is the result of recommendations set forth in the Township's 2007 Reexamination Report. The Reexamination Report states, "the redevelopment of existing land uses along Haddonfield Road between Route 70 and Chapel Avenue has occurred due to the secondary effects of the Garden State Park advancement...mobility and design guidelines will be examined in efforts of an overall corridor development."¹ This study is funded through a Transportation and Community Development Initiative (TCDI) grant administered by the Delaware Valley Regional Planning Commission (DVRPC).

2.0 EXISTING CONDITIONS

2.1 **STUDY AREA LOCATION**

As shown in Figure 1, the study area is located in the northwest section of Cherry Hill Township and is generally anchored by the former Garden State Park to the south and the revitalized Cherry Hill Mall to the north. Two major intersections are located within the study area: State Route 70 to the south and State Route 38 to the north.

Cherry Hill Township is a regional center of southern New Jersey and is located in northwestern Camden County, approximately seven miles east of the City of Philadelphia. Camden County has diverse land uses, ranging from the urban City of Camden along the Delaware River in the east to the natural open space of Wharton State Forest in the west, part of the Pinelands Reserve. The county consists of small towns, first generation suburbs, new suburbs, and rural/agricultural areas. The southeastern part of the county is dotted with family farms.

The Township is comprised of approximately 24 square miles of land area. It is bordered on the northeast by the municipalities of Maple Shade, Mt. Laurel and Evesham Townships of Burlington County; on the southeast by Voorhees Township; on the southwest by Haddon Township, Collingswood, and Haddonfield and Lawnside Borough; and on the northwest by Pennsauken Township and Merchantville Borough, all in Camden County. With its close proximity to

¹ Reexamination Report 2007, pg 34

Philadelphia and Camden urban centers, Cherry Hill is largely a metropolitan community. It is situated in the Metropolitan Planning Area (PA1) per the New Jersey State Development and Redevelopment Plan (SDRP).

The major roadways in Cherry Hill Township are the New Jersey Turnpike and Interstate 295, which run north-south through the center of the Township. These highways link the Township to other communities and parts of the state in a regional corridor extending from Delaware to New Jersey. Other major roads that have facilitated rapid growth of the Township are New Jersey State Highways Route 38 and 70, which provide east-west access.

Haddonfield Road (County Road 644), which is classified as a Principal Arterial in the Township's 2007 Reexamination Report, is a major thoroughfare that channels traffic north and south between the Borough of Haddonfield and Pennsauken Township. Haddonfield Road bisects the study area and separates the new residential and commercial development at the former Garden State Park site from the existing Locustwood residential neighborhood. Establishing a connection among the existing residences of Locustwood, the former Garden State Park and the commercial uses along the eastern side of Haddonfield Road, is fundamental and is examined in the study.

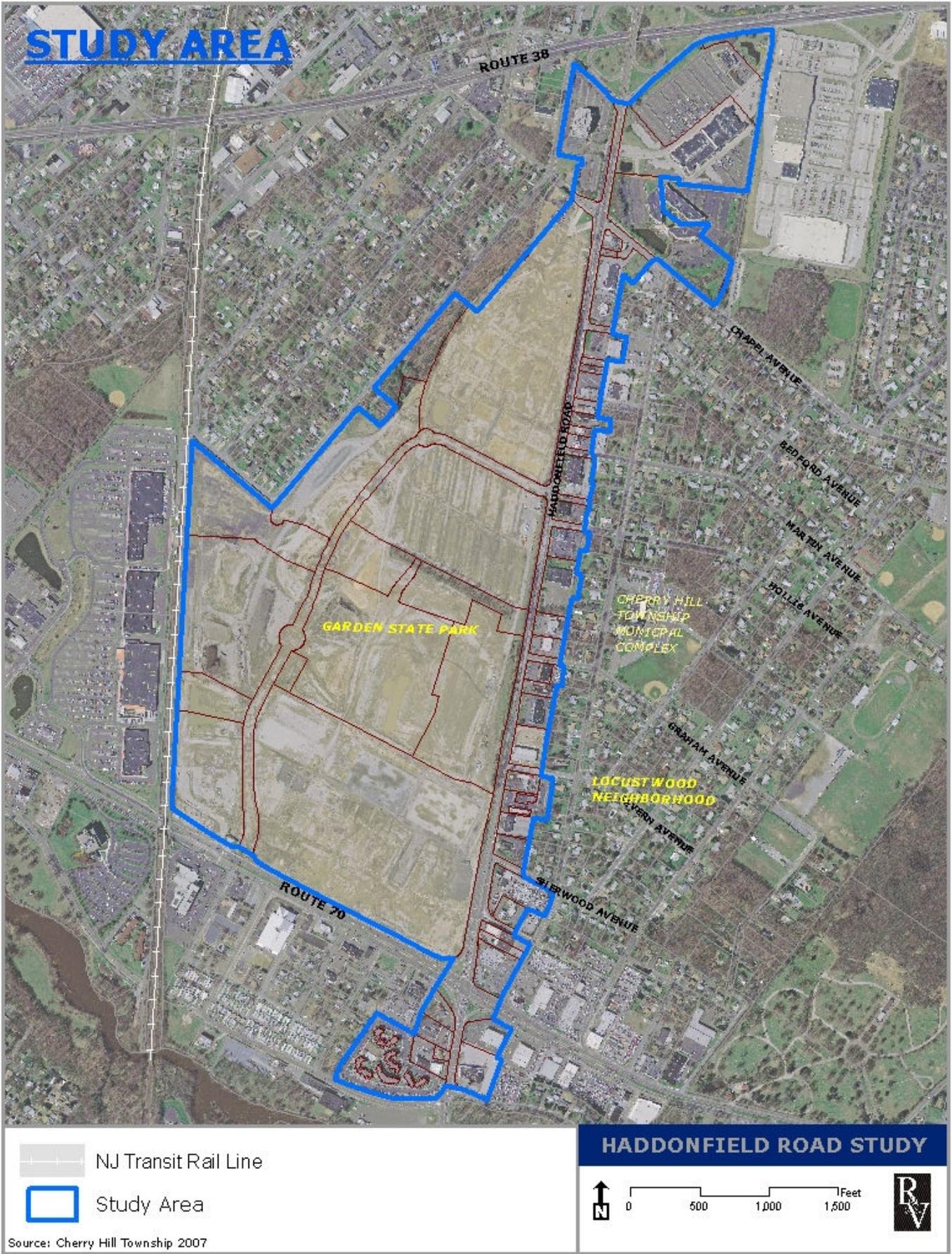


Figure 1

2.2 STUDY AREA DEMOGRAPHICS

The study area is situated within three census block groups. A block group is the smallest geographical unit that provides 100% data that is not statistically derived. The three block groups are:

- Census Tract 6032, Block Group 2
- Census Tract 6033.01, Block Group 1
- Census Tract 6033.01, Block Group 2

As shown in Figure 2, the block groups include the adjacent Locustwood residential neighborhood and the residential area located northeast of the former Garden State Park, which account for the majority of the demographic data discussed in Table 1. The land use pattern in the Locustwood residential neighborhood is considered a medium-density suburban layout, which supports a residential population of 5,348 persons. However, the residential component of Garden State Park will add a residential population of approximately 3,400 to the study area.

As shown in Table 1, there are approximately 2,084 households in the study area and 2,227 housing units indicating a vacancy rate of 7% within the study area. The study area’s vacancy rate is significantly higher than the Township’s (3.1%) and the study area contains over 16% of the vacant units in the Township.

Table 1.

	Haddonfield Road Study Area					Cherry Hill	
	Tract 6032, BG 2	Tract 6033.01, BG 1	Tract 6033.01, BG 2	Total #	Total %	Total #	Total %
Population, Race, & Ethnicity							
Population	1,541	2,984	823	5,348	100%	69,965	100%
White	1,372	2,026	741	4,139	77%	59,240	85%
African-American	68	220	25	313	6%	3,121	4%
Native American	3	3	1	7	0%	71	0%
Asian	37	606	38	681	13%	6,205	9%
Other	39	38	6	83	2%	515	1%
2 or more races	22	91	12	125	2%	813	1%
Hispanic	96	101	10	207	4%	1,778	3%
Housing Patterns							
Households	607	1,152	325	2,084	-	26,277	-
Average HH size	2	3	3	2.66	-	2.61	-
Housing Units	668	1,192	367	2,227	100%	27,074	100%
Occupied	607	1,152	325	2,084	-	26,227	-
Owner-Occupied	348	683	265	1,296	62%	21,761	83%
Renter-Occupied	259	469	60	788	38%	4,466	17%
Vacant	61	40	42	143	6%	847	3%

P3. RACE [71] - Universe: Total population
 Data Set: Census 2000 Summary File 1 (SF 1) 100-Percent Data

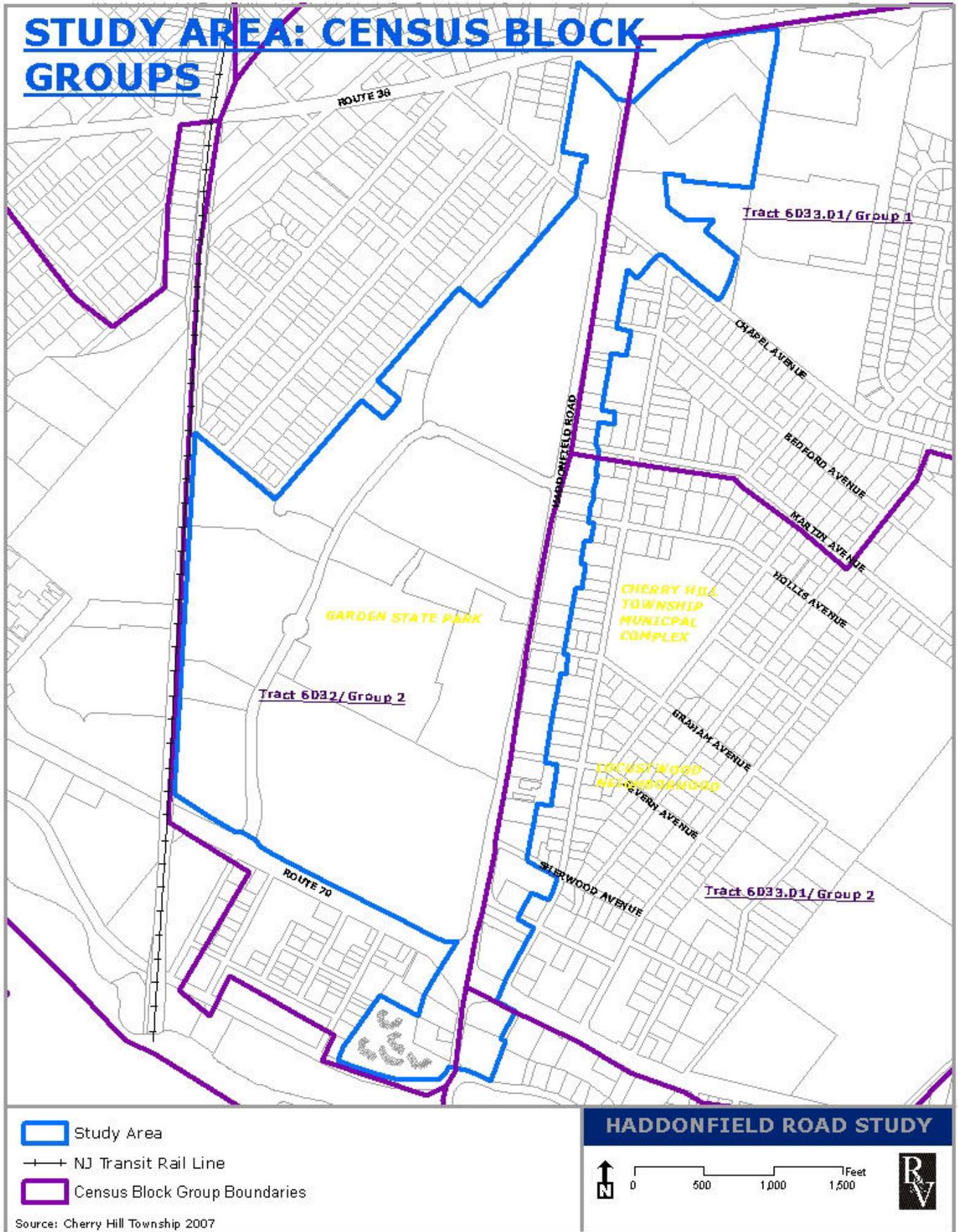


Figure 2

HADDONFIELD ROAD STUDY

CHERRY HILL TOWNSHIP • REMINGTON & VERNICK ENGINEERS

As shown on Table 2, the study area's median household income is \$45,051 which is less than the Township's median income of \$69,421. An analysis of commuting patterns reveals that the study area residents do not utilize public transportation, as almost 87% of study area residents commute via a private automobile. It appears that the majority of study area residents work in the immediate area; approximately 60% of the residents travel less than thirty (30) minutes to and from their place of employment.

Table 2.

	Haddonfield Road Study Area					Cherry Hill	
	Tract 6032, BG 2	Tract 6033.01, BG 1	Tract 6033.01, BG 2	Total #	Total %	Total #	Total %
Median HH Income	\$39,853	\$53,125	\$42,174	\$45,051	-	\$75,560	-
WORKERS: Means of Transportation to Work							
Car; truck; or van	486	1,230	329	2,045	100%	33,758	100%
Drove alone	474	1,026	286	1,786	87%	26,931	80%
Carpooled	12	204	43	259	13%	2,636	8%
Public transportation	36	43	12	91	4%	2,316	7%
Bus or trolley bus	0	8	0	8	0%	384	1%
Subway	36	16	12	64	3%	1,112	3%
Railroad	0	19	0	19	1%	812	2%
Bicycle	0	0	0	0	0%	25	0%
Walked	8	22	6	36	2%	425	1%
Other means	0	0	5	5	0%	97	0%
Worked at home	29	32	27	88	4%	1,308	4%
WORKERS: Travel Time to Work							
Workers (outside the home)	530	1,295	352	2,177	100%	32,450	100%
Less than 5 minutes	0	24	9	33	2%	644	2%
5 to 14 minutes	132	352	104	588	27%	8,047	25%
15 to 29 minutes	217	437	126	780	36%	11,439	35%
30 to 44 minutes	77	263	74	414	19%	6,208	19%
45 to 59 minutes	37	101	26	164	8%	2,846	9%
60 to 89 minutes	57	80	13	150	7%	2,283	7%
90 or more minutes	10	38	0	48	2.2%	983	3%

P3. RACE [71] - Universe: Total population

Data Set: Census 2000 Summary File 1 (SF 1) 100-Percent Data

2.3 STUDY AREA LAND USE

Eastern Side of Haddonfield Road

As shown below in Figure 3, the eastern side of Haddonfield Road is lined with a variety of automobile-oriented businesses, professional office uses, and neighborhood convenience retail uses. A land use survey was completed in September of 2007. The survey revealed that roughly 16% of the uses located on the eastern side are automotive related, such as car dealerships, automobile service centers, or automobile related retail uses.

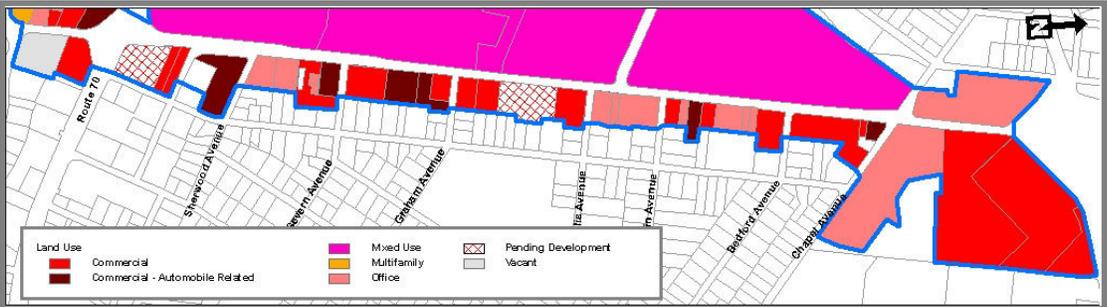


Figure 3

As illustrated in the images below, typically, the sites are designed to provide large open parking lots in the front or side yard of the structures, minimal landscaping, numerous free standing pole signs, and little to no pedestrian or bicycle infrastructure. The uses are somewhat disconnected and do not relate to each other in terms of use, site design, and site amenities.



Locustwood, the residential area behind to the eastern commercial uses contains an older housing stock that was constructed by individual developers. The neighborhood was individually developed parcel by parcel starting in the 1920's and continuing through the 1960's. The Cherry Hill Township municipal complex is situated in the center of the neighborhood.

A large parcel in the study area located on the southeastern corner of Route 70 and Grove Street consists of a vacant restaurant. The parcel is a prime location to establish a gateway into the Township. Redevelopment of this parcel should be considered. North of this parcel, constructed in 2003, is a drug store and pharmacy. At the northeast corner of Route 70 and Haddonfield Road, a proposal to reconstruct two restaurants has been approved by the Township. The reconstruction is the result of a fire on the site which destroyed the previous restaurants. The proposed reconstruction includes numerous site improvements, such as increased landscaping and sidewalk. Located south of the proposed restaurants are an office use and a retail use.

An automobile dealership occupies the entire frontage of the block between Wynnewood and Sherwood Avenues. The frontage of the next block is occupied by a medical center, office building, a retail use, a sports medicine facility, and a car wash. Continuing north along Haddonfield Road, a new retail center is situated on the corner of Severn Avenue and Haddonfield Road. North of the retail center, are auto-related commercial uses, specifically an automobile service center and an automobile parts retail use, and a small retail strip center. The block bound by Graham and Hollis Avenue consists of retail uses, a vacant building, and a restaurant. Plans for a retail center have been submitted for the vacant building.

Further north on Haddonfield Road, there are three office uses. A variety of uses are situated on the block bound by Martin and Bedford Avenues. Uses include an ice cream stand, animal hospital, car wash, office, antique store, and car rental agency. The animal hospital and the car wash have submitted plans for a renovation and redesign of the building and site. Continuing north, there is a fast-food restaurant with a drive-thru, a small retail strip center, and a gas station. Plans for a new restaurant to occupy the vacant restaurant have been approved and the retail center has submitted plans for an expansion. The last block of the study area is occupied by a large office complex. The buildings are multi-story.

Western Side of Haddonfield Road

The western side of Haddonfield Road is centered by the Towne Center development at the former Garden State Park. As illustrated on Figure 4, the overall development will include approximately 815,000 square feet of retail uses, 1,700 residential units, and roughly 74 acres of recreation and open space. Roughly 80,000 square feet of the retail uses and restaurants are located in the Towne Center. The remaining 735,000 square feet is a mix of big-box and medium-box retail uses located in the southern portion of the site.

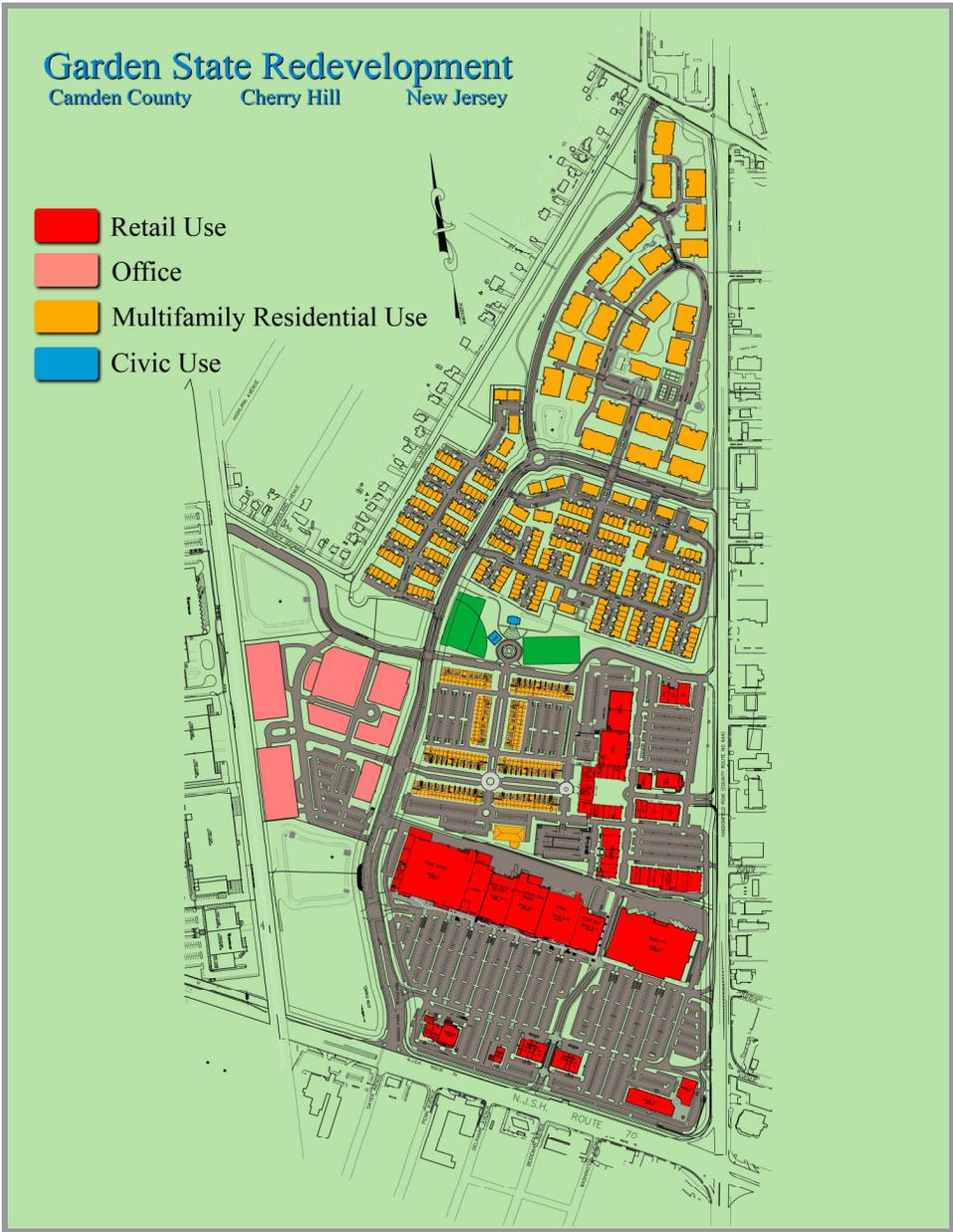


Figure 4

Nontraditional design principles, such as public gathering spaces, an extensive sidewalk and trail system, and on-site trolley service, are proposed to establish the feel of a downtown environment. Architectural elements throughout site, specifically in the retail and mixed-use buildings, include facades that give the appearance of multi-story buildings, featured towers, and four-sided brick on all structures. Public gathering spaces are located throughout the site and a large public plaza is proposed in the center of the site.



The Towne Center development is situated in close proximity to the Cherry Hill station on the New Jersey Transit Atlantic City Line. Adjacent to the rail station, office, and hotel uses are proposed. The station is located along the western boundary of the development. Residential units are planned above the retail uses in the Towne Center.

South of Garden State Park, situated on the block between Park Boulevard and Route 70 is a multi-residential development (Park Place), a beauty salon, an office use, a bank, and an auto-related commercial use. North of Garden State Park, situated on the block between Chapel Avenue and Route 38, is a multi-story Class A office building known as "Liberty View" (refer to Figure 5. Study Area: Land Use).

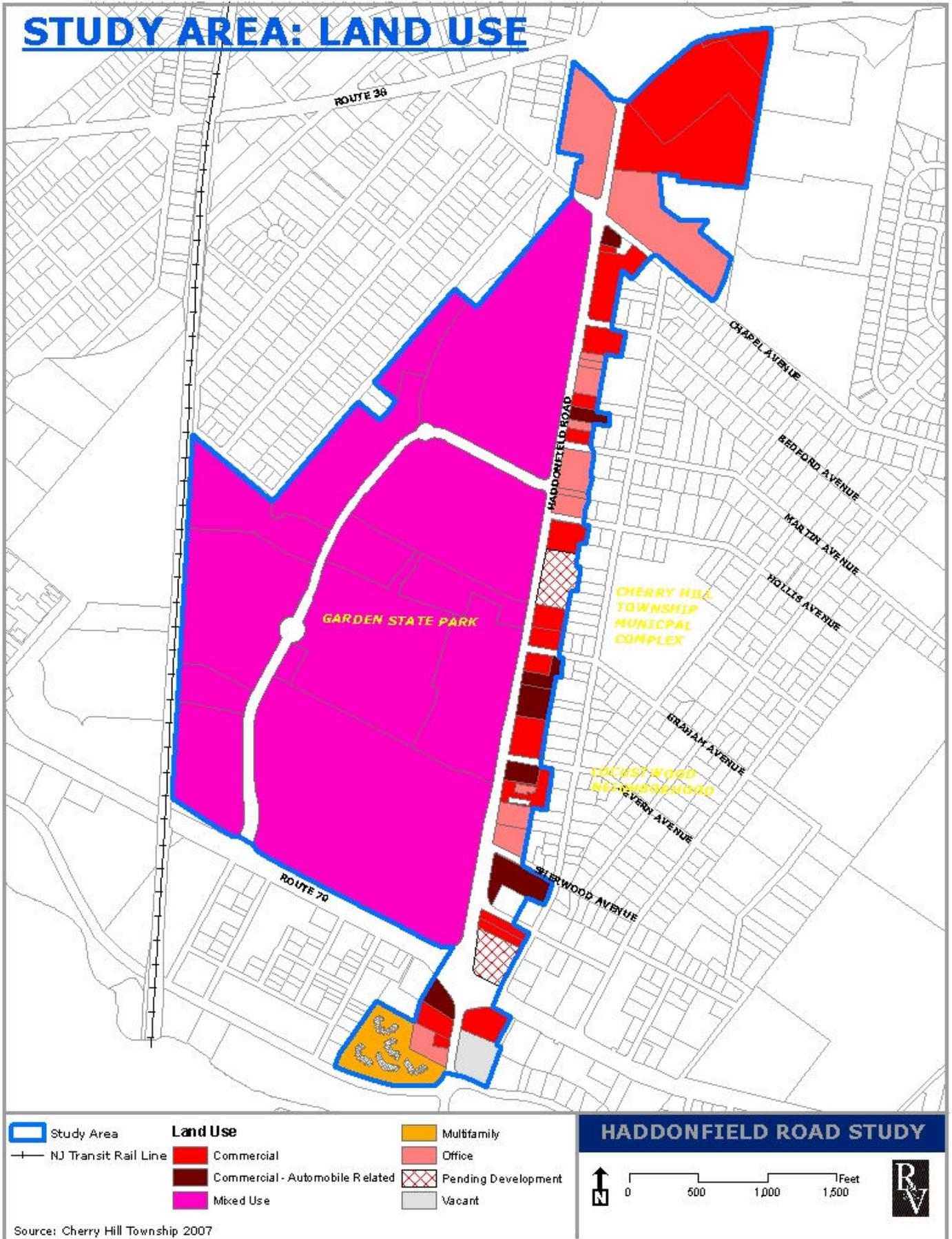


Figure 5

2.4 STUDY AREA ZONING

The Township is in the process of revising its zoning ordinance. Unless noted, the information contained below references the current 1976 ordinance.

Eastern Side of Haddonfield Road

As shown on Figure 6, the majority of study area properties along the eastern side of Haddonfield Road are within the Highway Business (B2) zone. The 2004 Township Master Plan recommended rezoning the Haddonfield Road corridor as B2 from B4 (Regional Business) Zone. The purpose of this zone is to provide uses that serve the needs of highway users or commuters, not necessarily the residents of Cherry Hill Township. The B2 zone is considered one of the most flexible zones in the Township. Various types of commercial uses are permitted including retail establishments, offices, banks, restaurants, and strip commercial centers. The proposed revisions to the B2 zone provide increased specificity on the types of commercial and retail uses permitted. The revisions also recommend automobile related uses, such as gas stations and automotive repair shops, as a conditional use instead of a permitted use.

The bulk and area regulations in the B2 zone are as follows:

LOT & YARD REQUIREMENTS	PRINCIPAL STRUCTURE	ACCESSORY STRUCTURE
Lot Size	20,000 square feet	Not applicable
Lot Frontage	100 feet (inside lot), 120 feet corner lot	Not applicable
Lot Depth	150 feet	Not applicable
Minimum Yard Depths, Setbacks, & Height Limitations		
Front Yard Setback	30 feet	Not permitted
Side Yard Setback	One 10 feet, Both 20 feet	5 feet
Rear Yard Setback	20 feet	10 feet
Max, Height	35 feet	10 feet

Special design requirements for uses within the B2 zone include providing access barriers through use of curb, planting strip or similar barrier. Additionally, the use of common access-ways by two or more permitted highway uses shall be provided to reduce the number and proximity of access points along the highway.

A portion of the study area situated at the northeast corner of the intersection of Haddonfield Road and Chapel Avenue is within the General Office (O2) zone. The intent of the O2 zone is to provide buildings for professional and business office uses. The proposed ordinance revisions recommend changing the intent of the O2 zone to encourage corporate office parks. The current ordinance permits office buildings on internal or corner lots sized at least 10,000 or 12,000 square feet, respectively. Currently, the Cherry Hill Commerce Center office complex

situated within the O2 zone consists of three buildings with a fourth building proposed.

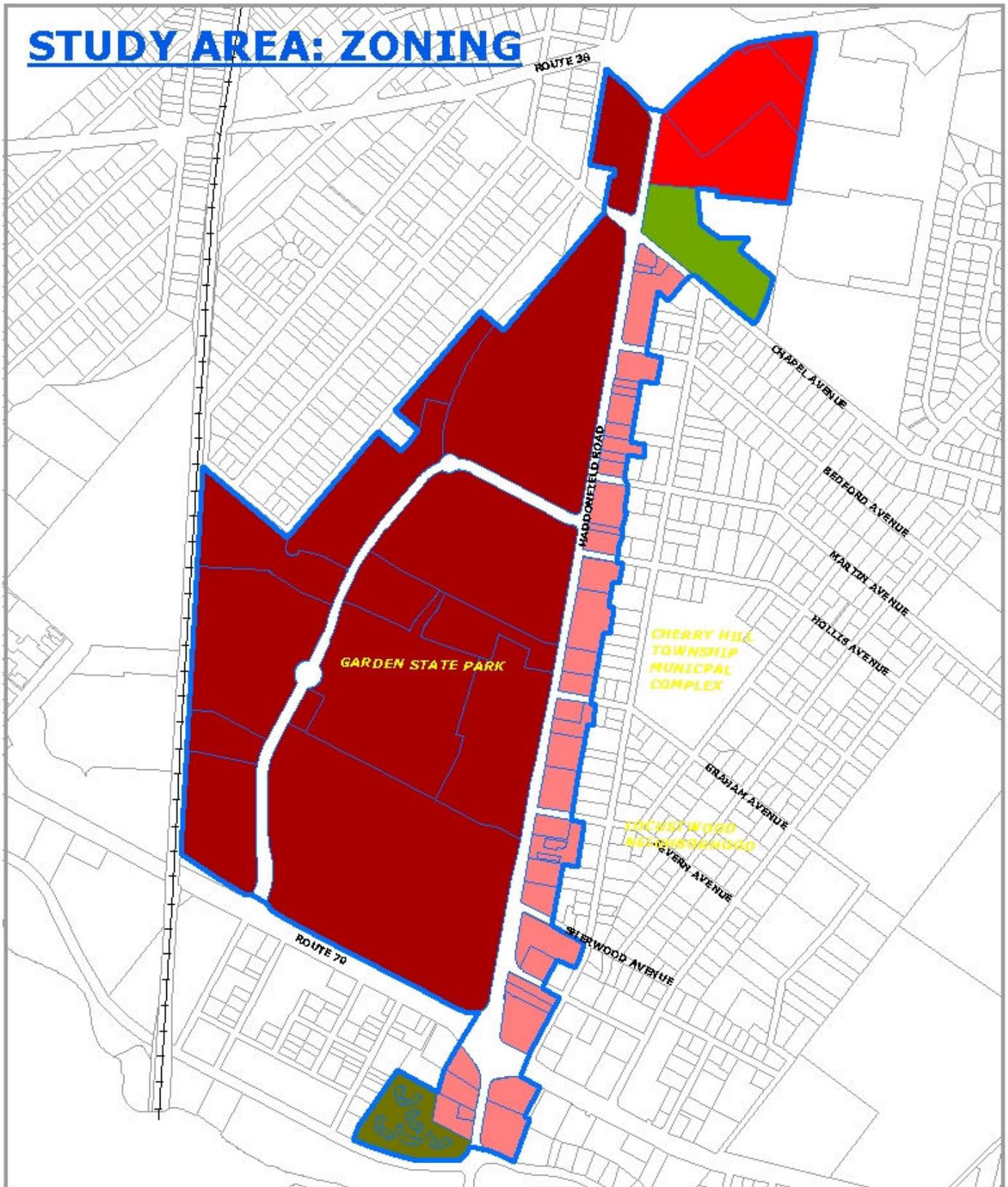
A small section of the study area located at the southeast corner of the Haddonfield Road and Route 38 intersection is within the Shopping Center (B3) zone. The purpose of this zone is to provide a wide range of business and service uses for Township residents. Uses permitted within this zone include but are not limited to: shopping centers, movie theaters, restaurants, professional offices, bakeries, banks, dry cleaners, and grocery stores. The minimum lot size is one (1) acre and the maximum height is 35 feet.

Western Side of Haddonfield Road

As shown on Figure 6, the parcels lining the western side of the study area are within the Regional Business (B4) zone. The entirety of the development at Garden State Park is classified as B4. The purpose of this zone is to provide an area for professional office, entertainment, and retail uses that serve the region. The regulations set forth in this zone are designed to encourage comprehensive planning and development of large areas. Uses permitted in the B4 zone include retail, restaurants, office, and various types of residential uses. The 2007 Reexamination Report states that this zone is overly complicated and recommends the Township take the necessary steps to simplify the zone requirements.

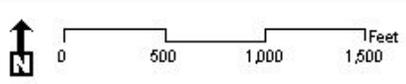
Parcels located at the southwestern corner of the study area are within the Multi-Residential (R10) zone. The R10 zone is designed to provide medium density development, such as townhomes and garden apartments, on lots at least 10 acres in size.

STUDY AREA: ZONING



Study Area	Zone Classifications	B4	O2
NJ Transit Rail Line	B2	R10	
	B3		

HADDONFIELD ROAD STUDY



Source: Cherry Hill Township 2007

Figure 6

2.5 STUDY AREA CIRCULATION

Haddonfield Road (C.R. 644) is classified as a Principal Arterial in the 2007 Reexamination Report. The road consists of four lanes (two lanes in each direction with a center turning lane) and is approximately 6.48 miles long. The study area contains 1.5 miles of Haddonfield Road and the speed limit is 30 miles per hour.

As a result of the development of former Garden State Park, new signals and traffic patterns have been added. A new signal and access point is located at Towne Center Drive, which is the main gateway into the center and there are additional curb cuts along the western side of the roadway. Throughout the planning process for Towne Center, the Township requested implementation of traffic calming and access management measures within the former Garden State Park to help promote safe pedestrian and bicycle trips. As discussed in detail below (*2.5 Study Area Planning and Development Activity*), the Township requested pedestrian signals, special road treatments at to pedestrian heavy intersections, and large landscaped medians.

The study area is adjacent to Cherry Hill Station for the New Jersey Transit Atlantic City Rail Line, which provides rail service between Philadelphia's 30th Street Station and Atlantic City. The line serves seven stations in New Jersey and one station in Philadelphia (30th Street Station). The line has 14 runs per day and averages 168 boardings. The Atlantic City Rail line also connects in Philadelphia to SEPTA regional rail service and the Northeast Corridor Service (via Amtrak) and in Lindenwold connects to PATCO service. The Cherry Hill Station is situated west of the study area, directly behind the Garden State Pavilions shopping center. The eastern side of the station extends into proposed office buildings and a hotel. The Township recently was awarded a grant to study the feasibility of Transit Oriented Development (TOD) at this location. It is anticipated that the proposed office building and hotel, which are adjacent to the residential and retail uses at the former Garden State Park, will be part of the TOD.

New Jersey Transit bus service provides bus service along Haddonfield Road and Chapel Avenue which services the study area. Additionally, the Cherry Hill Mall, which is situated slightly north of the study area, is a bus hub for NJ Transit bus routes #404 and #405. Additionally, New Jersey Transit has proposed new bus service along Haddonfield Road, Route #450, will begin service and will establish new bus stops, with bus shelters.

The 2007 Reexamination Report recommended that the development at the former Garden State Park connect with the Cherry Hill Bikeway (see Figure 7). The Township has submitted a proposal for a grant through the New Jersey Department of Transportation (NJDOT) Local Bikeway Program 2007 to enhance the existing infrastructure of the Cherry Hill Bikeway, upgrading existing sewer inlets with J-grade eco bicycle grates on roadways with existing Share the Road signage; provide thermoplastic bicycle lands where existing Share the Road signage is located; and to expand the bikeway to additional identified roadways. The Cherry Hill Bikeway is meant to provide recreational facilities for neighborhood residents, as well as connectivity between



Figure 7

neighborhoods and eventually connect with larger regional networks and destinations.

In addition to the bikeway system, the study area also accommodates a portion of a regional off-road trail system that connects Center City Philadelphia through the Cooper River Park system, to the Cherry Hill Mall. The system travels over the Ben Franklin Bridge and through the Camden Greenway. As part of the build-out of the former Garden State Park a pedestrian connection, which links to the regional system, will be constructed along Haddonfield Road. The trail will provide a needed link in completing a regional off-road bikeway and pedestrian system. As illustrated on Figure 8, there is one remaining parcel which needs to provide sidewalk along Haddonfield Road to complete the trail.



2.6 STUDY AREA PLANNING AND DEVELOPMENT ACTIVITY

As discussed above, the study area is in a period of rapid transition. The redevelopment of Garden State Park into a walkable community has spurred development activity along the eastern side of Haddonfield Road. Currently, the activity at the former Garden State Park is somewhat self-contained. In order for the “downtown” or “Main Street” feel to develop as desired by the Township as well as the retail components at Towne Center to succeed, there must be a seamless connection across Haddonfield Road, connecting existing development to the new development.

The Township has incrementally tried to improve the properties along the eastern side of Haddonfield Road. Over the past year, there have been numerous development or conceptual applications for parcels (refer to Table 3 and Figure 8).

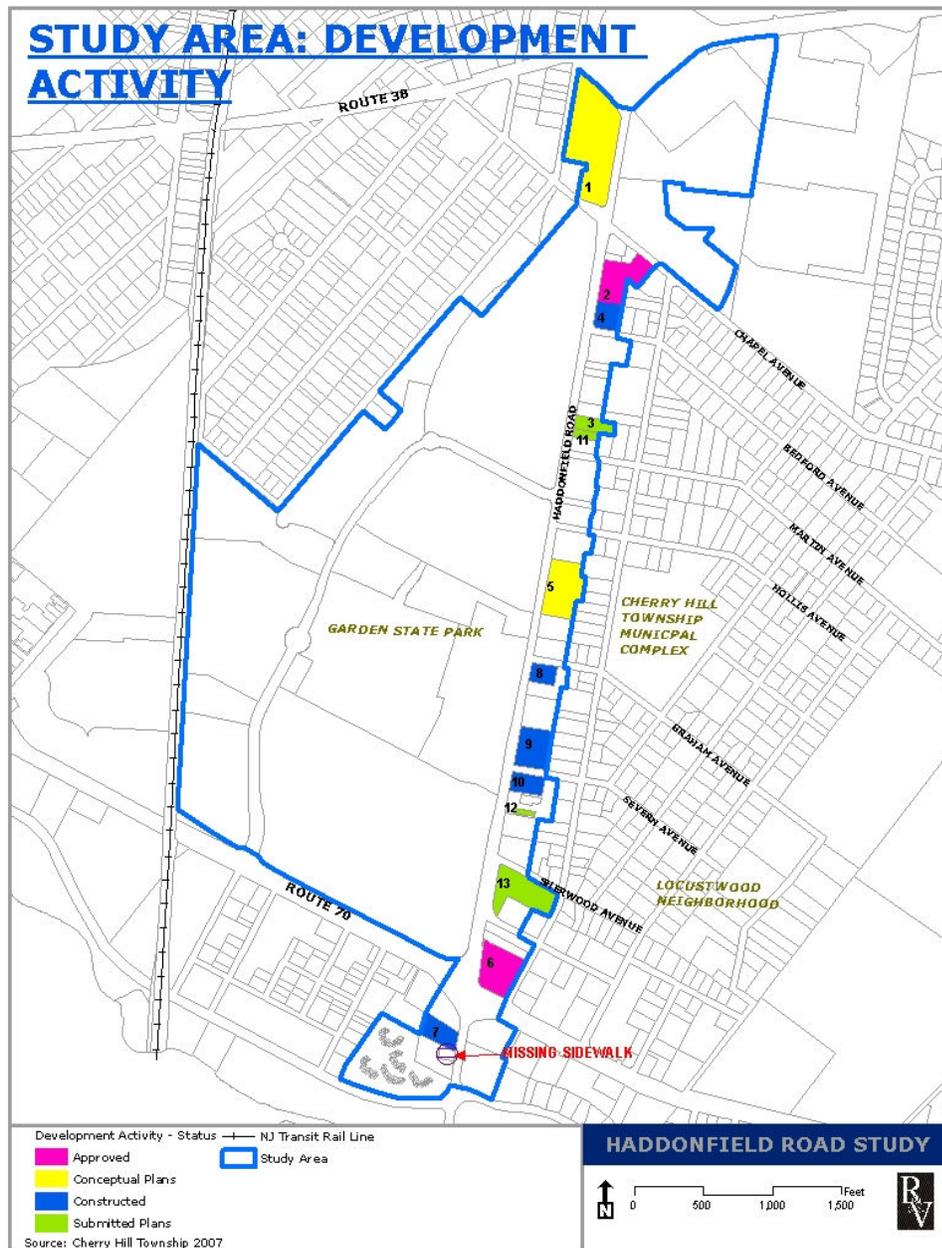


Figure 8

Key Number	Type of Improvement
1	Bank pad site with drive-thru facility
2	Construction of a 5,000 sq. ft. retail center
3	Addition and construction of a second story
4	Renovate existing vacant restaurant into fast-food restaurant with drive-thru
5	Construction of a retail strip center
6	Construction of two restaurants
7	Addition to existing bank
8	New pole-mounted sign
9	Construction of a retail center
10	New pole-mounted sign
11	Addition, new parking area, build canopy
12	New sign, possible site improvements
13	Construction of a 6,000 s.f. addition and site improvements

Due to the need for improvements of modern zoning regulations and design standards set forth in the Township’s ordinance to support similar development form as Towne Center, it is difficult for the Township to request the site improvements necessary to help establish a cohesive plan for the corridor. However, when the developer is agreeable or variances are required, the Township has been able to guide the site improvements to match the form or style of Towne Center. For example, 926 Haddonfield Plaza, represents several “neotraditional” elements, for example, the parking is located to the side of the building, a monument sign versus a pole sign is used, the architecture gives the appearance of two-stories, and amenities such as awnings, bike racks, sidewalks, and extensive site landscaping have been provided.





In addition, Commerce Bank recently completed an expansion that included the installation of colored pavers across the driveways, a walkway, and additional landscaping.

As a result of the redevelopment of Garden State Park, there have been roadway improvements including pedestrian and bicycle circulation improvements. In 2005, in response to plans submitted for Roadway and Traffic Signal Improvements for Haddonfield Road, Sherwood

Avenue to Chapel Avenue, by CMX Engineering/Schoor DePalma for the New Jersey Department of Transportation (NJDOT), the Township requested a minimum of six feet of grassy buffer between the sidewalk and the roadway curb with street trees and signage planted in the grassy buffer. The Township also stressed the importance of striped, laddered, thermoplastic crosswalks across Haddonfield Road at all driveways associated with the redevelopment at Garden State Park as well as larger crosswalks at Towne Center Drive and Garden Park Boulevard.

In 2006, the Township requested the re-striping of the existing crosswalks along Route 70, including the intersection of Haddonfield Road and Route 70. The Township stated that designated crosswalks will support a strong pedestrian environment. Thermoplastic-laddered design crosswalks were requested for the Haddonfield Road intersection to complement the existing pedestrian signal heads and strengthen the walkability of the area.

3.0 GOALS

To establish goals for this study, it was necessary to contact the business owners within the study area. On October 27, 2007, a survey of 22 questions was sent out to 34 businesses along Haddonfield Road. Of the 34 surveys sent out, seven were completed and returned providing a 21 percent response rate. The survey and a summary of the responses is included in Appendix A of this report.

The responses revealed that the business owners plan to make aesthetic and mobility improvements to their site; however, they have concerns with the Township's ordinance and approval process. Additionally, the business owners feel while Haddonfield Road is a desirable location, the high volume of traffic is unsafe and the overall appearance of the roadway is unattractive and not aesthetically pleasing.

The responses provided along with information obtained from Township officials created the foundation for the following goals:

- Establish mobility improvements to provide safe ingress and egress to study area properties and to ensure the safety of vehicles, pedestrians, and bicyclists.
- Provide design standards for landscaping, signage, pedestrian circulation, and parking that will enhance the study area but also are feasible for the business owners.
- Continue to capitalize on the redevelopment of Garden State Park and the new development at the Cherry Hill Mall by improving site aesthetics.

4.0 CIRCULATION ANALYSES

Haddonfield Road is a four-lane highway with three major intersections and a central turn lane along the expanse between New Jersey State Route 70 and New Jersey State Route 38. Based on the automatic traffic counts conducted by the Delaware Valley Regional Planning Commission (DVRPC) between December 5, 2006 and December 7, 2006, the average annual daily traffic (AADT) is approximately 29,865 vehicles. This means that on an average day, approximately 30,000 vehicles travel along Haddonfield Road. It has a posted speed limit of 30 miles per hour (MPH) and is a main connector between the Townships of Cherry Hill and Pennsauken. It also contains multiple areas of commercial and retail shopping while, at the same time, providing access for regional travelers.

Additionally, as discussed in Section 2.5, in the study area, there are other forms of transit that include the New Jersey Transit Atlantic City rail line and the New Jersey Transit Bus lines, which connects to the Westmont Port Authority Transit Corporation (PATCO) Station

There is concrete sidewalk located on the eastern side of Haddonfield Road, except between Route 70 and Wynwood Avenue, and a bituminous path is located on the westerly side of Haddonfield Road adjacent to the redevelopment at the former Garden State Park. There is a pedestrian crossing at the Wynwood Avenue intersection with Haddonfield Road; however, the crosswalk markings are unobvious to pedestrians. The ambiguous crosswalk "WALK" signal conflicts with the drivers' green light for left turning movements from Wynwood Avenue onto Haddonfield Road, and based on the lack of crosswalk markings, those same drivers are unaware of the possibility of pedestrians crossing in this area.

Due to the recent redevelopment of the Garden State Park site, traffic volumes are expected to increase therefore it is anticipated that patrons destined for this area may seek alternative means to access the businesses along this corridor. Public transportation is ideal for decreasing the congestion on the roadways; however, it does not guarantee that there is an easy way to travel to the stores the consumer might wish to visit. The departure and arrival schedules might not be ideal for the consumer as well. Another transportation option to get to the stores is walking. As a result of the lack of crosswalks and the entrance/exit aprons to the various parking lots within the study area, the pedestrians have

some problems getting to their destinations. They have to walk a long way to get to crosswalks to cross Haddonfield Road and they have to constantly be aware of where and when vehicles may be entering or exiting the numerous parking lots along Haddonfield Road. These reasons could deter consumers from walking or using public transportation.

To help formulate recommendations for circulation improvements, two sample blocks were chosen. To ensure consistency throughout this report, the same sample blocks are used for analysis in Sections 5.0 and 6.0 below. Along Haddonfield Road, the two sample blocks chosen are between Severn Avenue and Hollis Avenue (see Figures 9 and 10). Between these intersections, there are a number of ingress/egress driveways for the retail stores located on the eastern side of the road. Based on the automatic traffic counts conducted by the DVRPC between Towne Center Road and Martin Avenue, traveling northbound on Haddonfield Road, the AM peak hour is between 11AM and 12 noon and the PM peak hour is from 12 noon to 1PM. Traveling southbound within the same limits, the AM peak hour is from 8AM to 9AM and the PM peak hour is from 4PM to 5PM.

After a site visit, it was observed that the sample blocks between Severn Avenue and Hollis Avenue have some structural components that can be altered to resolve some of the traffic issues. Recently, there has been an addition of one intersection to the former Garden State Park between the sample blocks which may increase pedestrian crossings from the two sides of Haddonfield Road. This provides pedestrian movements between the east and west side, but it also impedes the through movements along Haddonfield Road. Additionally, some of these driveways can be eliminated to avoid the turning conflicts along Haddonfield Road. Moreover, some of the parking lot layouts can be restructured to employ the concept of shared parking; eliminating these driveways and reorganizing the parking lot layouts decrease the amounts of curb cuts and increase the interior movements in the parking lots.

Another necessity is to increase the pedestrian movements throughout the Haddonfield Road shopping area. Improving the accessibility to cross Haddonfield Road and developing shared parking will enhance the appeal to walk to the assorted stores as well as decrease the need to drive between the parking lots, which will lessen the traffic movements in that area. Also, more sidewalks and/or bicycle paths should be added connecting to the residential communities to encourage pedestrian movements to and from the retail stores. Adding sidewalks to improve movement to the retail area from the various transit stations in the area will also increase the possibility of shopping from far away locations without adding unnecessary vehicles to the roadways.

Lastly, integrating the diverse non-vehicular transit into future development plans along Haddonfield Road should be considered. Ensuring that there are ample sidewalks and pathways between the rail lines and the various bus routes will encourage those patrons in the surrounding areas to utilize these transit options. This will also result in a decrease of traffic volume along this corridor.

5.0 PLAN OPTIONS – EXISTING RETAIL CONDITIONS

5.1 **SAMPLE BLOCKS**

As discussed above in 2.0 Existing Conditions, the study area is currently in the midst of a period of market-driven “redevelopment”. Many of the properties along the eastern side of Haddonfield Road have submitted plans for expansions and various types of site improvements. A goal of this study is to develop recommendations to help create a seamless connection between the former Garden State Park and the surrounding uses, through design and mobility.

To help formulate recommendations, two sample blocks within the study area were identified and studied to determine what types of improvements are needed to create an aesthetically pleasing and pedestrian friendly site.

The two blocks identified are located on the eastern side of Haddonfield Road between Severn Avenue and Graham Avenue and between Graham Avenue (Figure 9) and Hollis Avenue (Figure 10). The blocks were selected because they represent typical development patterns and the combination of uses provides opportunities for shared parking and the creation of internal circulation networks.



Figure 9

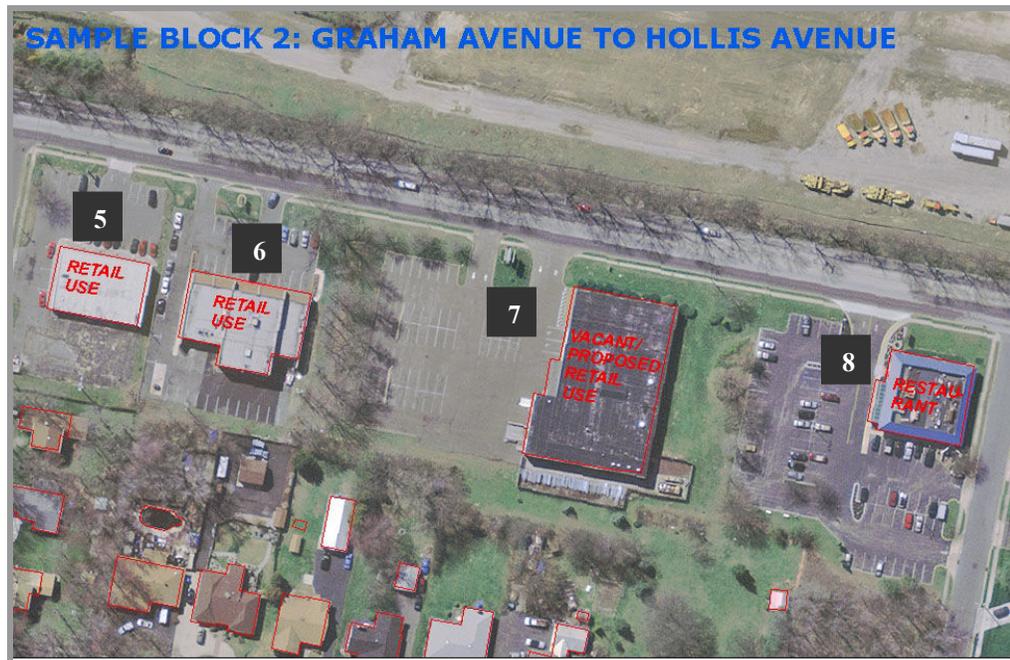


Figure 10

Figures 9 and 10 above illustrate the amount of impervious coverage on each parcel, the lack of defined internal circulation patterns, and the frequency of curb cuts along Haddonfield Road. Along the two sample blocks, there are five retail strip-style centers; however, one of the retail centers contains only one business and another is a combination of a retail business and an auto-repair facility. There is also a restaurant, another auto-repair facility, and a vacant structure. Table 4, below, provides specific information on the use occupying each parcel.

5.1 PARKING EVALUATION

To evaluate the parking provided with each use, it was necessary to approximate the square footage of each building, in accordance with Township ordinance standards. Digital data provided by the Township was utilized to estimate the dimensions of each structure.

The following list of properties coordinates with the numbers provided on Figures 9 and 10. Please note the number of parking spaces identified includes parking spaces in the rear and sides of the property as well as "informal" (not-striped) spaces.

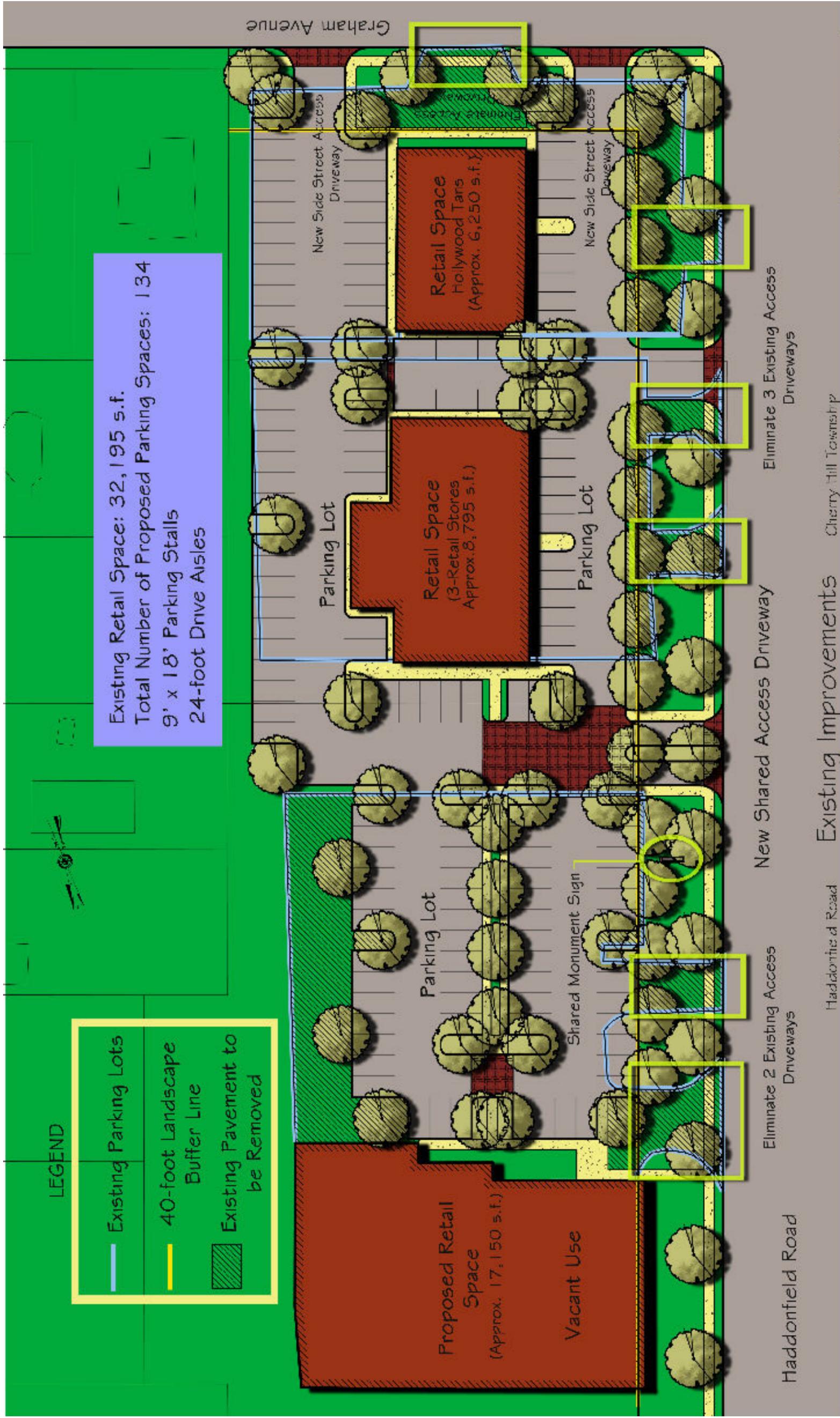
Table 4.

#	Appx. Bldg. Size (s.f.)	Use	# of Parking Spaces
1	14,168	Newly constructed retail strip center (926 Haddonfield Road)	52
2	10,789	Automobile service center and automobile and motorcycle supply store.	43
3	10,291	Automobile service center/body shop (Rodi's Body Shop)	9
4	8,716	Retail strip center	24
5	6,425	Retail strip center with one use (Hollywood Tans)	42
6	8,795	Retail strip center	36
7	17,150	Vacant building (proposed retail strip center)	36
8	6,867	Newly remodeled restaurant (Onasis Restaurant)	74

The Township's requirements for off-street parking are primarily calculated using "gross leasable area". The "gross leasable area" was not available for this analysis; therefore, the square footage of the building was used to approximate the amount of required parking. Field investigations revealed that the parking provided along the sample blocks varies. For example, #5, which is a Hollywood Tans, has 42 parking spaces, which appears to be excessive since there is a limited number of tanning beds and only two to three employees at each shift. However, the parking provided for #3, an auto-repair facility, appears to be insufficient given that customer's cars to be repaired are parked somewhat haphazardly within the site and tend to utilize the rear parking lot of the neighboring use. Additionally, it appears that the parking provided for many of the properties along the two sample blocks does not conform to the requirements set forth in the Township ordinance. For example, #8, Onasis Restaurant, has 74 parking spaces; however, the Township ordinance requires 8 parking spaces for every 1,000 square feet of dining area. Therefore, the maximum number of parking needed is 55 spaces. Also noted was a lack of consistent parking lot design standards including, landscaping, screening, parking islands, etc.

The recommendations set forth in Section 4.0 (Circulation Analyses), the input from the business owners along the property, and field investigations conducted as part of this study, support consideration of shared parking among the various uses. Shared parking, not only, allows for a decrease in the amount of impervious coverage and more efficient use of a site but also reduces the number of curb cuts along Haddonfield Road and encourages internal pedestrian circulation.

Figure 11 illustrates a conceptual shared parking layout for the block between Graham Avenue and Hollis Avenue. The proposed concept is for existing development and utilizes most of the existing buildings. To improve circulation along Haddonfield Road, five access points have been reduced to two. 134 parking spaces are proposed which accommodates the existing uses on site and allows for some shared parking between Hollywood Tans and the adjacent retail space.



LEGEND

- Existing Parking Lots
- 40-foot Landscape Buffer Line
- Existing Pavement to be Removed

Existing Retail Space: 32,195 s.f.
 Total Number of Proposed Parking Spaces: 134
 9' x 18' Parking Stalls
 24-foot Drive Aisles



Shared Parking Concept

Existing Improvements Cherry Hill Township

Figure 11

DESIGN ALTERNATIVES – EXISTING CONDITIONS

Four alternatives for the streetscape improvements to the sample blocks are provided; however, to produce a connected streetscape the Township selected Concept 4 as the "*Preferred Alternative*" for implementation. The Preferred Alternative will be the guide for developing the ordinance standards set forth in Section 7.0 of this report.

Streetscape Alternatives – Existing Conditions**Concept 1 – Illustrated in Figure 12**

- Four (4) foot grass buffer.
- Five (5) foot concrete sidewalk.
- Forty (40) foot landscape buffer (dimensioned from curbline) along Haddonfield Road frontages.
- Twenty five (25) foot landscape buffer (dimensioned form curbline²) along side frontages.

Concept 2 – Illustrated in Figure 13

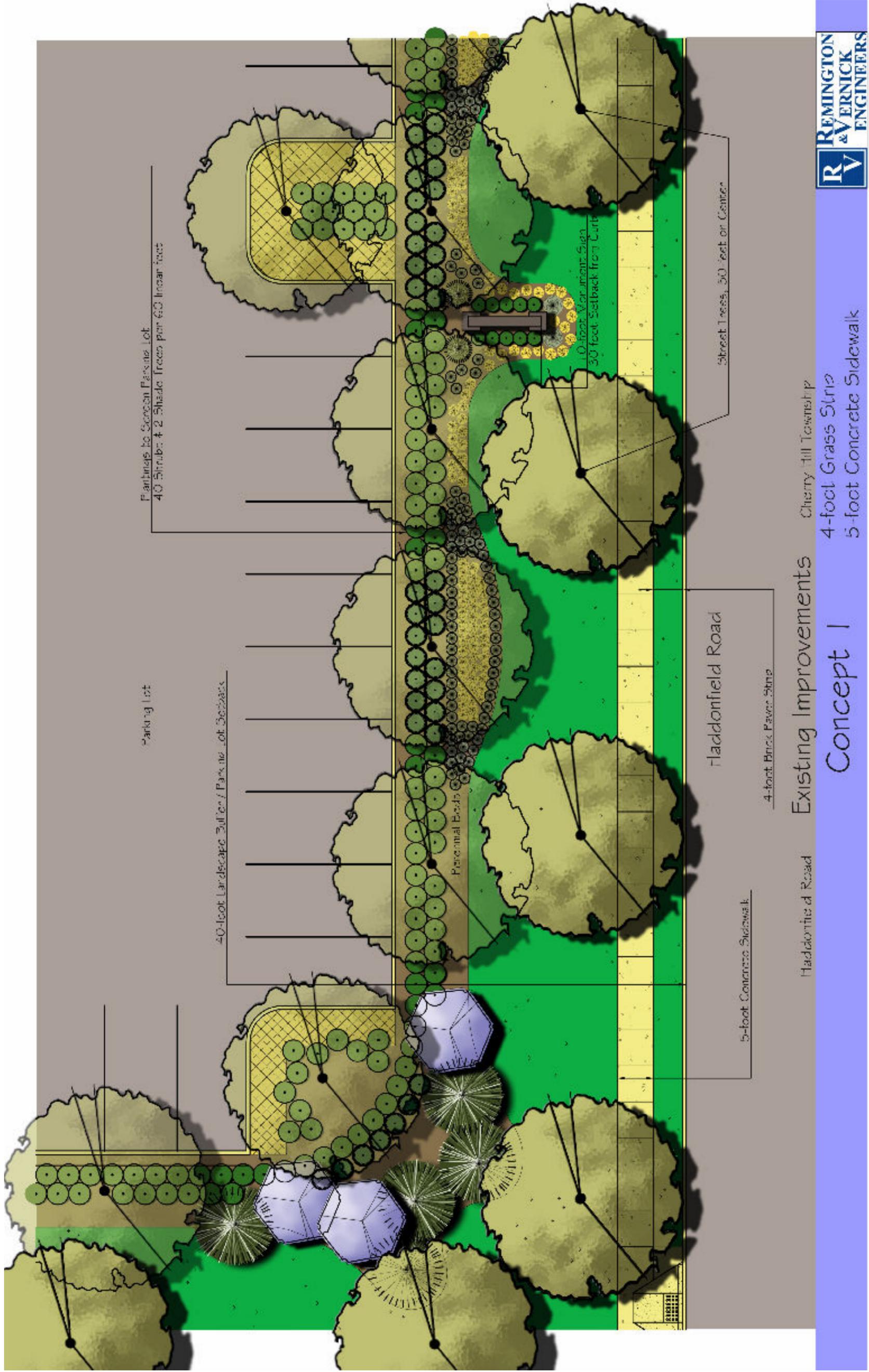
- Four (4) foot decorative brick paver strip.
- Five (5) foot concrete sidewalk.
- Forty (40) foot landscape buffer (dimensioned from curbline) along Haddonfield Road.
- Twenty five (25) foot landscape buffer (dimensioned form curbline) along side frontage.

Concept 3 – Illustrated in Figure 14

- Two (2) foot decorative brick paver strip.
- Five (5) foot planting strip with street trees planted fifty (50) feet on center and massed with shrubs, groundcover plants, perennials and/or ornamental grasses.
- Four (4) foot decorative brick paver or concrete sidewalk.
- Forty (40) foot landscape buffer (dimensioned from curbline) along Haddonfield Road.
- Twenty five (25) foot landscape buffer (dimensioned form curbline) along side frontage.

Concept 4 (Preferred Alternative) – Illustrated in Figure 15

- Two (2) foot concrete strip along curb.
- Four (4) foot decorative brick paver strip and/or planning island.
- Six (6) foot concrete sidewalk.
- Forty (40) foot landscape buffer (dimensioned from curbline) along Haddonfield Road.
- Twenty five (25) foot landscaped buffer (dimensioned from curbline) along side frontage.



Existing Improvements Cherry Hill Township
 4-foot Grass Strip
 5-foot Concrete Sidewalk

Concept 1

Figure 12

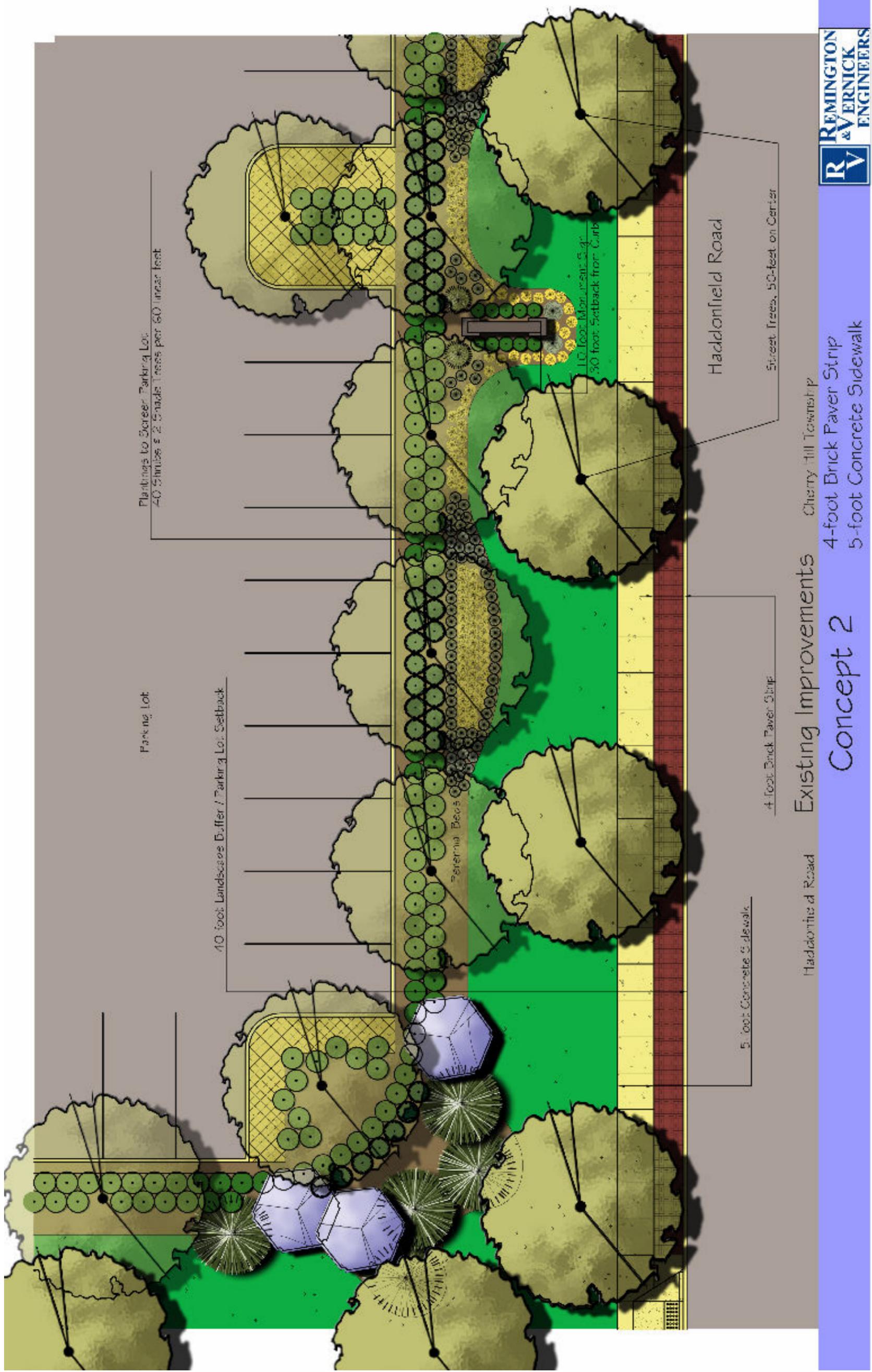


Figure 13

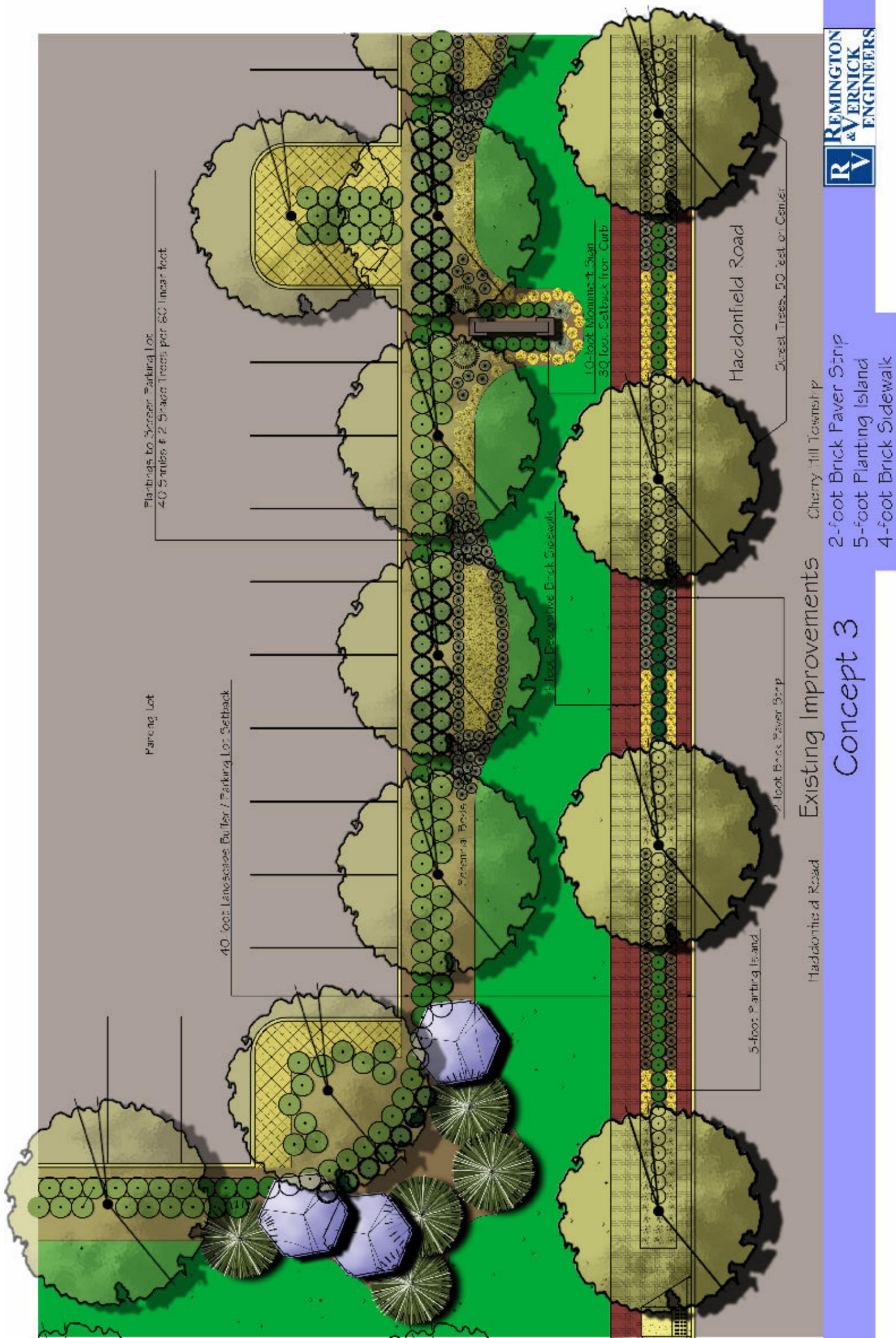


Figure 14

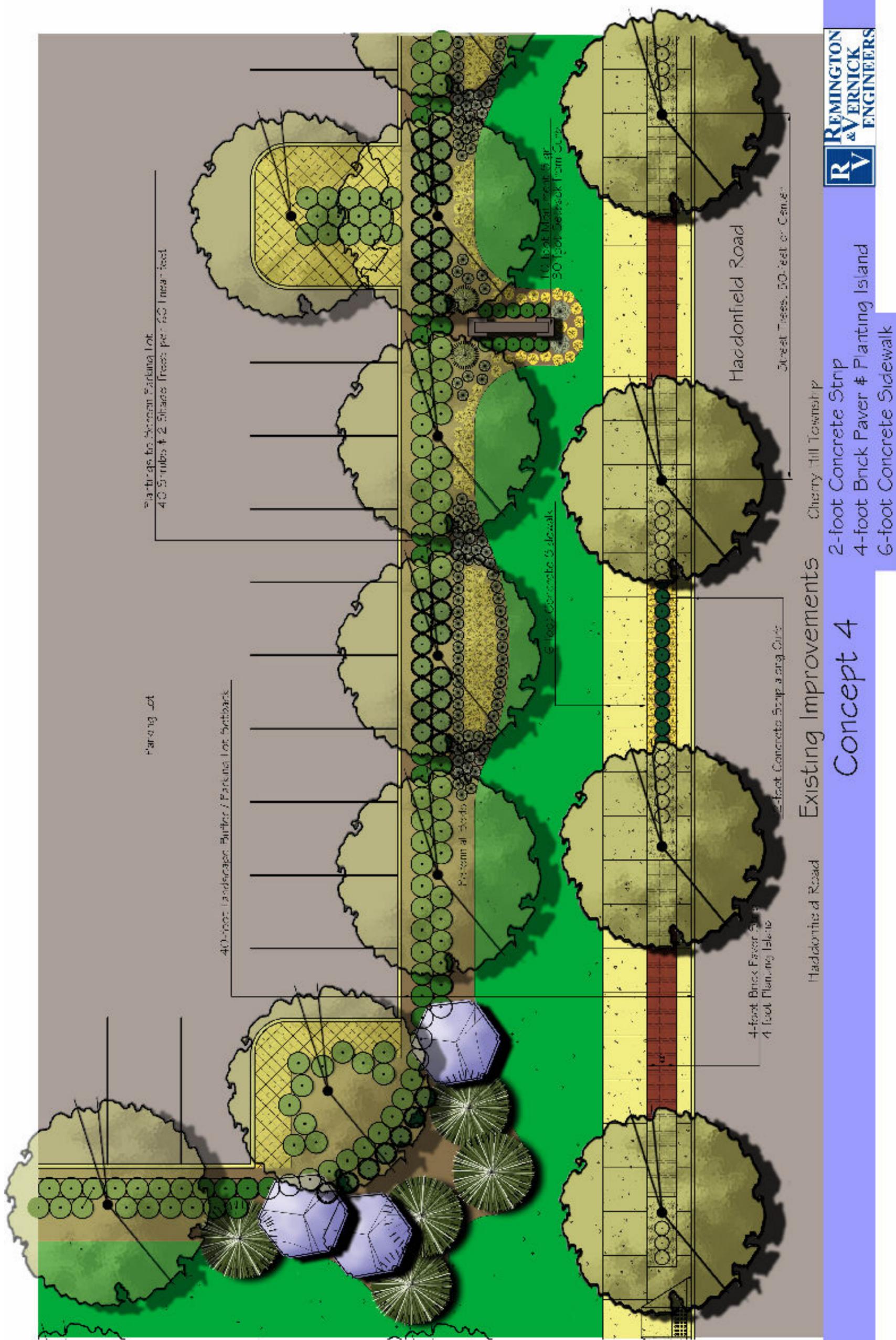


Figure 15

6.0 BEST ALTERNATE

Two optional development concepts were prepared as “Best Alternates” for the sample blocks. The concepts are applicable to new construction assuming that new building footprints are constructed and shared parking and circulation systems are utilized. The proposed concepts below reflect the site design standards set forth in Section 7.0.

6.1 *Proposed Commercial Site Concept 1*

As shown on Figure 16, Concept 1 proposes 56,134 square feet of commercial space. The buildings are oriented toward the primary pedestrian means of access with secondary entrances facing other means of access such as parking lots. Pedestrian circulation among the various buildings is strongly encouraged through the use of decorative crosswalks and sidewalks. Shared access driveways are utilized rather than providing a means of ingress and egress for each structure.

Four of the proposed buildings are situated approximately 25 feet from the curb and provide a consistent street wall along Haddonfield Road. The center building (5,880 square feet) is setback further from the road to mimic the development at the former Garden State Park and to allow for the creation of a significant access way, through landscaping and decorative paving, from Haddonfield Road.

Pursuant to the Township ordinance parking requirements for a neighborhood convenience center less than 75,000 sq. ft is 1 parking space for 1,000 square feet of G.L.A.. 224 parking spaces are required². Concept 1 proposes 225 9' x 18' parking stalls. Three loading spaces, which meet the proposed new ordinance requirement of 12' x 40' when site is serviced by vans or trucks with less than four (4) axles, are provided to serve each grouping of buildings.

6.2 *Proposed Commercial Site Concept 2*

As shown on Figure 17, Concept 2 proposes 54,588 square feet of commercial space. Similar to Concept 1, the buildings are oriented toward the primary pedestrian means of access with secondary entrances facing other means of access such as parking lots. All of the buildings are oriented towards Haddonfield Road creating a consistent street wall. The buildings are approximately 25 feet from the curb.

Pedestrian circulation among the various buildings is strongly encouraged through the use of decorative crosswalks and sidewalks. Shared access driveways are utilized rather than providing a means of ingress and egress for

² Since G.L.A. is not available for conceptual analysis, the square footage was used to determine parking needs.

each structure. When accessing the site from Hollis Avenue, the vehicular circulation pattern allows for one-way access through the entire site.

Pursuant to the Township ordinance, 218 parking spaces are required³. Concept 2 proposes 222 9' x 18' parking stalls. Two loading zones, which meet the proposed new ordinance requirement of 12' x 40' when site is serviced by vans or trucks with less than four (4) axles, are provided to serve each grouping of buildings. Neither of the loading zones provides direct access to the center building (6,900 square feet). It is anticipated that loading for the uses in this building will be provided through smaller vans and trucks and most likely they will utilize the parking stalls adjacent to the building when unloading materials. It is assumed that to ensure customer safety and an pleasurable shopping experience, each tenant will establish a loading schedule that will not interfere with park customer periods.

³ Since G.L.A. is not available for conceptual analysis, the square footage was used to determine parking needs.

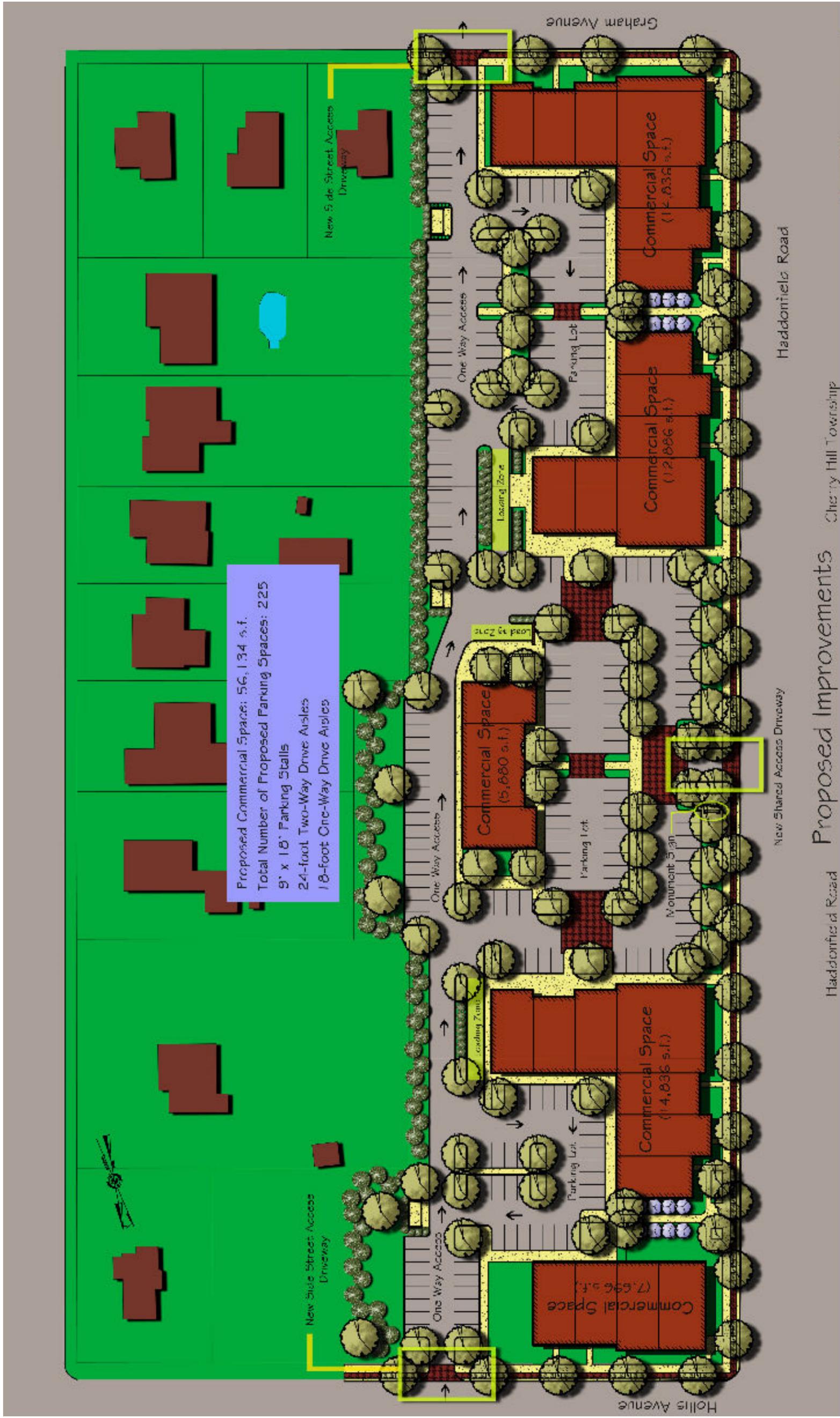


Figure 16



Figure 17

7.0 CODE EVALUATIONS AND RECOMMENDATIONS

To achieve the concepts presented in this report an overlay zone along Haddonfield Road is recommended. The overlay zone encompasses the entire study area and advances the concepts set forth in Section 5.0 and 6.0. The following ordinance is proposed for Section 428 of the Township’s Land Development Ordinance.

Section 428. HADDONFIELD ROAD OVERLAY ZONE

- A. Specific Intent.** The Haddonfield Road Overlay (HRO) zone is intended to encourage mobility improvements as well as to implement design standards to support a seamless transition from the Towne Center at Garden State Park across Haddonfield Road.
- B. Permitted Uses.** All of the uses permitted in the Highway Business (B2) zone set forth in Section 417B.1-25.
- C. Accessory Uses and Structures Permitted.** All of the accessory uses and structures permitted in the Highway Business (B2) zone set forth in Section 417C.1-9.
- D. Conditional Uses Permitted.** All of the conditional uses permitted in the Highway Business (B2) zone set forth in Section 417D.1-3.
- E. Area, Yard, Height and Building Coverage.** Except as otherwise modified, the following bulk standards shall apply to all buildings in the Haddonfield Road Overlay (HRO) zone.
 - 1. Except as otherwise modified the following bulk standards shall apply to all development which utilizes the **existing building footprint** in the Haddonfield Road Overlay (HRO) zone.

LOT & YARD REQUIREMENTS	PRINCIPAL STRUCTURE
Minimum Lot Size	20,000 s.f.
Minimum Lot Frontage	Inside: 100'; Corner: 120'
Minimum Lot Depth	150'
Minimum Yard Depths, Setbacks, and Height Limitations	
Front Yard	25'
Side Yard	One 20'; Both 40'
Rear Yard	35'
Maximum Height	35'
Parking Area Setback/Landscape Buffer	
From street curblines	40'
From residential zones	12'
From non-residential zones	5'
From side street curblines	25'

- 2. Except as otherwise modified the following bulk standards shall apply to all **new development or development creating new building footprints** in the Haddonfield Road Overlay (HRO) zone.

LOT & YARD REQUIREMENTS	PRINCIPAL STRUCTURE
Minimum Lot Size	20,000 s.f.
Minimum Lot Frontage	Inside: 100'; Corner: 120'
Minimum Lot Depth	150'
Minimum Yard Depths, Setbacks, and Height Limitations	
Front Yard	All buildings shall be setback 25'. If a multiple building development, the maximum setback can be exceeded if 75% of the other buildings have a 25' setback (refer to Figure 1).
Side Yard	25'
Rear Yard	12'
Maximum Height	35'
Parking Area Setback/Landscaped Buffer	
From street curbline	25'
From residential zones	12'
From non-residential zones	5'
From side street curbline	25'



Figure 1

F. Parking Area Location and Design

1. ***For new development or development creating new building footprints***, parking shall be provided in the rear and the side of buildings. If parking in the front of buildings must be provided, it shall not occupy more than 25 percent of the lot frontage. Direct pedestrian connections shall be provided from the rear and side parking areas to the front of all buildings. Whenever possible, breaks in the building footprints should be provided to allow pedestrian access from rear parking areas to building fronts. The pedestrian accessway should be a minimum of twenty-five (25') feet.
2. Curb cuts along Haddonfield Road should be kept to a minimum. Parking areas in the rear of buildings shall be interconnected with lots on adjacent properties to permit accessibility between properties and to minimize the frequency of egress and ingress onto Haddonfield Road.
3. Rear parking shall be accessed by means of common driveways and through side streets where possible.
4. Vehicular entrances shall be strategically placed to minimize disruption with pedestrian circulation as well as the number of curb cuts.
5. The perimeter and interior of parking lots shall be equipped with sidewalks for safe pedestrian circulation.

G. Streetscape

For ***all developments***, the following standards for streetscape shall be utilized (refer to Figure 2):

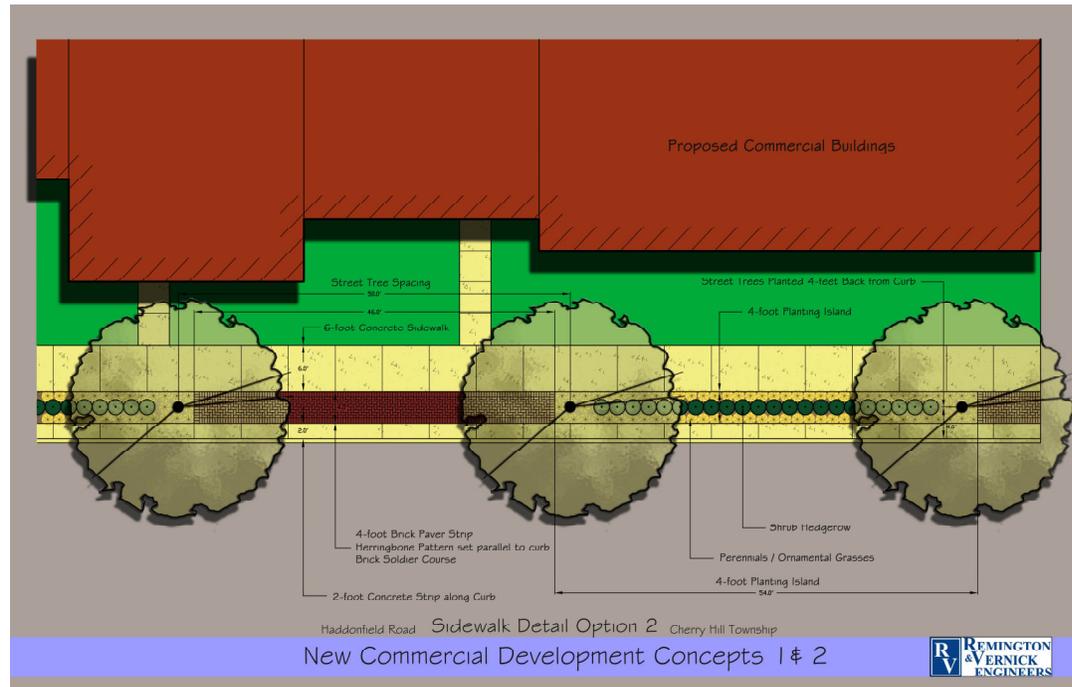


Figure 2

1. A two (2') foot concrete strip along curb.
2. An alternating pattern of a four (4') foot decorative brick paver strip (herringbone pattern with brick soldier course) and a four (4') foot landscaped planting strip shall be provided. In the landscaped planting strip evergreen/deciduous shrubs, ground cover plants, perennials and/or ornamental grasses not exceeding a height of two (2') feet at mature growth shall be utilized.
3. Rather than providing an alternating pattern as described in (b), there is the option to provide a decorative brick paver strip (herringbone pattern with brick soldier course) or a landscaped planting strip. If the four (4') foot decorative brick paver strip (herringbone pattern with brick soldier course) option is utilized, street trees shall be planted in four (4') feet by four (4') feet tree pit openings within the brick paver strip (refer to Figure 3). The landscaped planting strip shall be planted as described in (b).

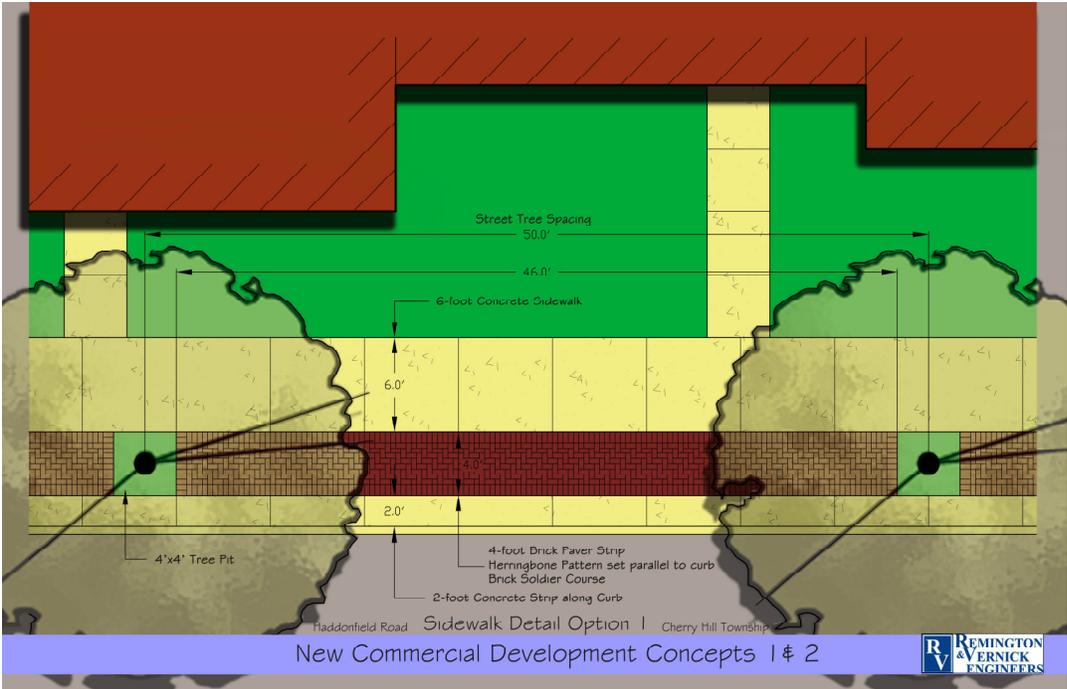


Figure 3

- 4. A six (6') foot concrete sidewalk.
- 5. Street trees shall be provided at fifty (50') foot intervals on center, four (4') feet from the curb centered within the four (4') foot planting strip.

H. Landscaping

- 1. Street or Shade Trees
 - a. For all development, a minimum of ten (10) shade or street trees per acre, not including parking lot trees, should be provided onsite.
 - b. Street or shade trees, except for those existing, preserved, or transplanted, shall be at least ten (10') feet in height, balled and burlapped, when planted, and have a minimum caliper of three (3") inches. All trees shall be nursery stock and free of insects and disease.
 - c. Street trees shall be spaced fifty (50) feet on center along the entire length of all road frontages.

- 2. Off-Street Parking Areas

1. Off-street parking areas shall be buffered with evergreen plant material that grows to a mature height of at least three (3) feet along all frontages to screen parked cars from roadway, headlight glare, adjacent residential properties, and/or public view.
2. The parking lot buffer shall be planted at a rate of forty (40) shrubs and two (2) deciduous shade trees per sixty (60) linear feet of parking lot perimeter. Perennials, groundcover plants, and/or ornamental grasses should be provided at a rate of fifty (50) plants per sixty (60) linear feet. A maximum of five (5) different species of perennials, groundcover plants and/or ornamental grasses may be used. Trees provided around the parking lot perimeter shall be planted five (5) feet from the pavement edge and spaced thirty (30) feet on center. The parking lot buffer must be planted in one of the following manners:
 - (i) A formal layout with shrubs planted in a double staggered hedgerow three (3) feet on center. In the hedgerow, a maximum of three (3) different shrub species grouped in large plant masses may be utilized per sixty (60) linear feet of parking lot perimeter. Perennials, groundcover plants and/or ornamental grasses should be planted in masses groupings in front of the hedgerow along the roadside frontage.
 - (ii) A naturalistic layout with large groupings. A minimum of five (5) shrubs of the same species in one massed grouping. A maximum of six (6) different shrub species may be planted per sixty (60) linear feet of parking lot buffer. Evergreen trees and ornamental/flowering trees may also be provided within the naturalistic landscape parking lot buffer but will not reduce the required number of shade trees around the parking lot perimeter.
3. Parking lot islands should be provided at a rate of one (1) parking lot island planted with one (1) deciduous shade tree per every ten (10) parking spaces. Parking islands shall be a minimum of nine (9') feet in width by eighteen (18') feet in length or equal to the size of one parking space.
4. Parking lot islands shall be provided at the end of any double row of parking spaces. The size of the islands shall be nine (9') feet in width by thirty-six (36') feet in length or equal to the area of two parking stalls. Each parking lot island at the end of a double parking row shall have two (2) deciduous shade trees per parking lot island.
5. Parking lot islands shall be massed with plant material not to exceed two (2) feet high at a mature growth including groundcover

plants such as liriopse or spreading junipers, perennials, and/or ornamental grasses. The plant masses should cover a minimum of seventy-five (75%) percent of the total parking island area.

I. Buffers

1. The buffer area/parking area setback between residential and nonresidential uses shall be twenty-five feet. This buffer area shall be planted in a manner to create a 100% impervious evergreen visual barrier. A double staggered row of evergreen trees spaced a maximum of twelve (12') feet on center planted at a height of six (6') to eight (8') feet high at time of planting shall be provided. Additional intermittent shade trees and evergreen/deciduous shrubs should be provided within this buffer area.
2. The buffer area/parking area setback between residential and nonresidential uses maybe reduced to 12 feet if a six (6') foot wooden or vinyl privacy fence is provided. A minimum of fifty (50%) percent of the buffer area shall be landscaped with evergreen trees, shade trees, and shrubs.

J. Signage

1. All freestanding signs within the HRO zone shall be monument signs.
2. All signs shall not exceed fifteen (15) feet in height.
3. The maximum sign area shall conform to the requirements set forth in Section 513.Q.
4. For development utilizing the **existing building footprint**, the sign shall be located within the forty (40') foot landscape buffer, thirty (30') feet from the curbline.
5. For **new development or development creating a new building footprint**, the sign shall be located five (5') feet behind the sidewalk.

K. Off-Street Parking

Entrance/Exit Drives. Driveways shall be no closer than thirty (30') feet of any residential zone.

L. Building Massing

1. Buildings should avoid long, uninterrupted wall or roof planes. Building wall offsets, including both projections and recesses, shall be required, in order to provide architectural interest and variety, and relieve the visual effect of a single, long wall. Similarly,

roofline offsets should be provided along any roof measuring longer than seventy-five (75) feet in length in order to provide architectural interest and variety to the massing of a building and relieve the negative visual effect of a single, long roof.

2. Larger buildings should reduce the appearance of bulk through recesses, offset, changes of plane, stepped terraces and other devices which break down and articulate building mass.

M. Building Location and Orientation

1. Spatial relationships between buildings and other structures should be geometrically logical and/or architecturally formal.
2. Buildings should be located to allow for adequate fire and emergency access.
3. In a multiple building development, buildings located on the interior of a site should front towards and relate to one another, both functionally and visually. To the extent possible, multiple building developments should be organized around features such as courtyards, quadrangles, which encourage pedestrian activity.

N. Storefronts

1. Storefronts are an integral part of a building. The building's façade should dictate the storefronts composition in terms of spacing, and alignment of buildings and windows. Storefronts should be designed to be compatible with the overall character of the façade, to maximize pedestrian interest, and to maintain a pedestrian scale.
2. Buildings with multiple storefronts should be unified through the use of architecturally compatible styles, colors, details, awnings, signage, and lighting fixtures on all storefronts.

O. Interpretation

Unless otherwise stated above, all requirements of Article V are applicable in the HRO zone.

8.0 CONCLUSION

The Haddonfield Road Study has been timely, in order to improve the recent inertia of redevelopment from the expansion and revitalization of the former Garden State Racetrack and Cherry Hill Mall. This corridor was identified due to need and opportunity to apply mobility and land use controls to facilitate and ease redevelopment. The recent activity of site improvements has led to the consequential expansion of Haddonfield Road to include a center turning lane, further hindering pedestrian movement across the road between commercial and residential areas.

The effectiveness of this Study relies on property owner involvement, developer cooperation, and the promotion of principles and overall implementation of the action items. Therefore, the following steps are outlined to successful implementation of the Haddonfield Road Study:

1. **Distribution.** The promotion and acceptance of the Study is critical to achieving the identified goals; therefore the following approaches should be done within a short time frame:
 - Post the Study on the Cherry Hill Township website (www.cherryhill-nj.com) in its entirety available for download in .PDF format.
 - Send a letter outlining the availability of the Study on the website for review to property owners along the corridor via Certified Mail, as well as via regular mail to tenants and business owners.
 - Issue a similar letter to stakeholders and collaboration partners, including the Cherry Hill Business Partnership, Camden County Planning Board, Camden County Improvement Authority, Cherry Hill Regional Chamber of Commerce, South Jersey Chamber of Commerce, Cherry Hill Mall (PREIT), Garden State Park owners and tenants, Delaware Valley Regional Planning Commission, New Jersey Department of Community Affairs, and similar entities.
 - Work with staff from the Mayor's Office for press releases and general promotion of the Study.
2. **Business Owners Association.** The Township encourages and can provide assistance to the formation of a Haddonfield Road Business Owners Association, possibly in partnership with the Cherry Hill Business Partnership.
3. **Beautification.** In conjunction with the various divisions of the Township, recommendations and assistance for "quick fix" improvements to the structures and sites could be provided, such as landscaping, signage, ADA standardization, sidewalk improvements, etc. Furthermore, zoning and property maintenance enforcement could be targeted to key trouble areas.
4. **Overlay Zone.** The adoption of the recommended overlay zone code, with any required modification, should be integrated into the existing and working draft of new Development Control Ordinance.
5. **Funding.** The Township should work with NJDCA, CCIA, and DVRPC to identify grants and resources for improvement, as well as work with local bank branches for low-interest loans. Potential funding sources include:

- Private donations by larger business owners within the Study Area.
 - Municipal Local Aid: It provides state and federal aid pertaining to a variety of transportation-related issues, including pedestrian safety, bikeways, centers of place, and others. *New Jersey Department of Transportation (NJDOT)*
 - Business Relocation Assistance Grant Program (BRAG): It provides relocation grants to businesses that create a minimum of 25 new full-time jobs by relocating to New Jersey or expanding to a new location in New Jersey. *New Jersey Department of Commerce*
 - Workforce Training Grants & Other Assistance: It awards matching grant dollars to employers for classroom-based and on-the-job training. *New Jersey Department of Commerce*
6. **Expansion.** The Township has solicited contributions from private stakeholders to expand the study area further northward, between Route 38 and Church Road (C.R. 616), in order to determine needs and focus improvements and resources. Additional studies, such as the Golden Triangle Visioning Study, which studies the feasibility of enhancing the existing Cherry Hill station of the NJ Transit Atlantic City Rail Line and adjacent land uses to create a Transit-Oriented Development (TOD).

The Haddonfield Road Study ensures future growth along this corridor attains the high standards of the adjacent redevelopment of key properties along Haddonfield Road. The Haddonfield Road corridor is becoming one of the major retail corridors as a result of the redevelopment of the Garden State Park and Cherry Hill Mall. The Township would like to direct these market forces to comprehensively improve all of the properties along Haddonfield Road.

APPENDIX A

HADDONFIELD ROAD STUDY



October 26, 2007

Dear Merchants:

The Township of Cherry Hill is dedicated to providing a high-quality living environment for our residents and business owners. We are constantly looking for ways to improve our community and retain our national reputation as "one of the best places to live".

As part of this effort, we are conducting a study along the Haddonfield Road commercial corridor, between Route 70 and Chapel Avenue, which transports thousands of people through Cherry Hill on a daily basis. The Township was awarded a grant from the Delaware Valley Regional Planning Commission (DVRPC) to examine existing mobility and design conditions along the corridor and recommend improvements.

Please take a few minutes to complete the enclosed survey and return it in the provided stamped envelope no later than MONDAY, NOVEMBER 19th. Due to the limited amount of businesses along this corridor, each survey will be evaluated individually and taken into consideration. We value your opinion and appreciate your input. Please do not hesitate to contact Nicole Hostettler, Senior Planner, with any questions at nhostettler@chtownship.com or (856) 661-4737.

Sincerely,

David Benedetti, PP, AICP
Director
Department of Community Development

enclosure (survey)



On October 26, 2007, a survey of 22 questions was sent out to 34 businesses in the Haddonfield Road corridor. Of the 34 surveys sent out, seven were completed and returned providing a 21 percent response rate. The following is an inventory of all the responses that were received from the Haddonfield Road Study:

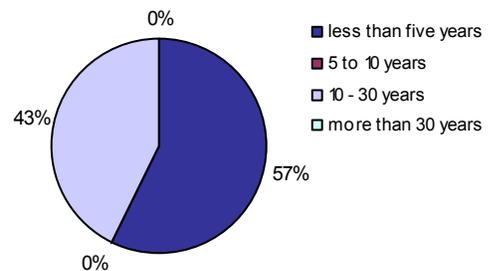
SURVEY QUESTIONS

1. Do you own a business or property along Haddonfield Road?

- 7 yes
- 0 no

2. If yes, how long have you owned the property?

- 4 less than five years
- 0 between five and ten years
- 3 between ten and 30 years
- 0 more than 30 years



3. Why did you choose to locate your business or purchase property at this location?

- (respondents could choose more than one)*
- 7 location
 - 1 affordability
 - 0 community reputation of Cherry Hill
 - 0 other

4. What is the nature of the business or property?

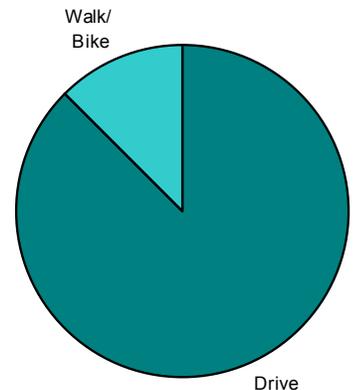
- 4 Commercial Retail
- 0 Personal Service
- 3 Office
- 0 Other (Specify):

5. Approximately how many employees do you have at this location?

- 6 employees
- 8 employees
- 10 employees
- 10 employees
- 15 employees
- 16 employees
- 25 employees

6. How do these employees generally commute to work?

- 7 Drive
- 0 Public Transit
- 1 Walk/Bike
- 0 Other



7. How many curb cuts (entrances and exits) onto Haddonfield Road does the property have?

- One business had two curb cuts
- 6 properties had one curb cut

8. Approximately how many parking spaces does your business or property have at this location?

- 10 parking spaces
- 12 parking spaces with additional parking in the rear
- 16 parking spaces
- 30 parking spaces
- 40 parking spaces
- 50 parking spaces
- 120 parking spaces

9. Do you feel there is adequate parking in the site?

- 7 yes
- 0 no

10. Does the parking lot on your site connect to the parking lot of an adjacent site?

- 2 yes
- 5 no

11. If no, do you feel this would benefit your site? Why or why not?

- "No, have adequate parking!"
- "We have adequate parking"
- No answer
- "Yes, it would allow extra parking in case needed"
- "No"

12a. Does the site have sidewalks?

- 7 yes
- 0 no

12b. Does the site have crosswalks?

- 1 yes
- 4 no
- 2 no answer

12c. Does the site have bike racks?

- 0 yes
- 5 no
- 2 no answer

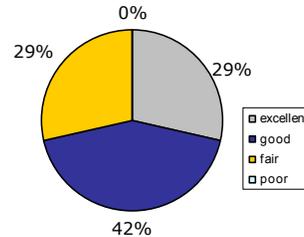
12d. Does the site have outdoor seats?

- 0 yes
- 5 no
- 2 no answer

Please rate the following aspects of Haddonfield Road, between Route 70 and Chapel Avenue, as it relates to your site?

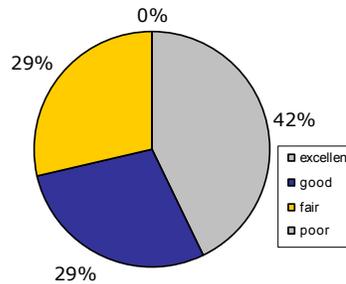
13a. Access your site?

- 3 excellent
- 2 good
- 2 fair
- 0 poor



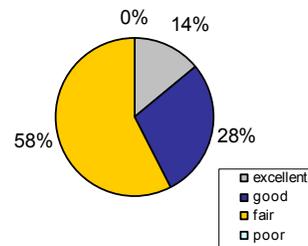
13b. Access out of your site?

- 2 excellent
- 3 good
- 2 fair
- 0 poor



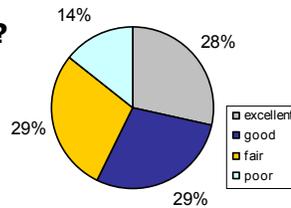
13c. Access between sites?

- 1 excellent
- 2 good
- 4 fair
- 0 poor



13d. Volume of traffic?

- 2 excellent
- 2 good
- 2 fair
- 1 poor



14. What traffic or design improvements do you think are needed along Haddonfield Road?

- 4 surveys had no response
- "None"
- "Unsure"
- "Some at our intersection at this time"
- "Road signs have to be allowed bigger"

15. Have you made improvements to your property?

- 5 Yes
- 2 No

15a. If so, what improvements?

- "Landscaping, painting (?), new gutters"

- "Signage and painting"
- "Repaired walkway and parking lot, replaced sign"
- "Paint"
- "Cosmetic"



EXISTING CONDITIONS



ALTERNATIVE CONDITIONS

16. Do you plan to make any improvements to your property in the next five years?

- 4** Yes
- 3** No

16a. If so, what?

- "Additional signage, replacement of exterior walls and wood treatment"
- "Resurface parking lot"
- "Possible 2nd Floor addition and reconfigure parking"
- "Yes, exterior cosmetics and better commercial space for each unit (interior improvement)"

17. Why have you or will you make improvements?

- (2) Redevelopment area (i.e. Cherry Hill Mall, Garden State Racetrack, etc.)
- (3) Customer/ Client Demand
- (1) New Business/Tenant Fit-Up
- (2) No answer

18. How do you think the Township of Cherry Hill can help?

- "To relax some of pressures of latter and directory signs"
- "Approve the plans"
- "??"
- "Taking it easy with the ordinances"
- "No answer"

PHOTO COMPARISON

19. What do you like about the existing conditions?

- (2) "No answer"
- "Few traffic lights; easy access in and out of businesses"
- "Access from both conditions"
- "4-Lane highway allows for traffic volume"
- "Functional but not aesthetically pleasing"
- "None"

20. What do you not like about the existing conditions?

- (2) "No answer"

- "Commercial appearance"
- "Difficulty of exiting and entering our business"
- "Unattractive, high volume of traffic unsafe"
- "Not aesthetically pleasing"
- "Too plain"

21. What do you like about the alternative scenario?

- (2) "No answer"
- "Has a more "Main St., USA" appeal"
- "Attractive"
- "Pretty, Main streetish, trees"
- "It is aesthetically pleasing"
- "More upscale, better for business, attract traffic"

22. What do you not like about the alternative scenario?

- (2) "No answer"
- "Encroachment of greenery on roads and walkways where space may already be limited"
- "Does not have access from both (4) lanes"
- "Unrealistic traffic volume"
- "Concerned with traffic flow"
- "None"